

Wantirna Health Precinct West

Comprehensive Development Plan

August 2023



Department
of Transport
and Planning

HOW TO READ THIS INCORPORATED DOCUMENT

The Wantirna Health Precinct West (WHPW) Comprehensive Development Plan (CDP) guides land use and development outcomes for the WHPW and should be read in conjunction with the provisions of Schedule 3 to Clause 37.02 Comprehensive Development Zone (CDZ) of the Knox Planning Scheme.

All future development of the WHPW must implement this CDP, including the Vision, Objectives, Requirements and Guidelines of this CDP.

The way in which the various elements of the CDP are to be applied is as follows:

- **Vision and Objectives:** All future development must implement the vision and objectives of the CDP.
- **Future Urban Structure:** All future development is to implement the Future Urban Structure of the WHPW as shown on Plan 2. Variations may be permitted by the Responsible Authority, provided the overall vision and objectives for use and development of the WHPW are achieved.
- **Requirements:** All requirements of this CDP must be met. Where they are not demonstrated in a permit application, requirements may be included as a condition on a planning permit. A requirement may reference a plan, table or figure in the CDP.
- **Guidelines:** All guidelines should be met. If the responsible authority is satisfied that an alternative approach will achieve the outcomes sought by the guideline, then the responsible authority may adopt the alternative approach. A guideline may include or reference a plan, table or figure in the CDP.





CONTENTS

1. Outcomes	1
1.1 Vision.....	1
1.2 The precinct.....	1
1.3 Sub-precincts	1
1.4 Strategic content	1
1.5 Objectives.....	2
2. Implementation	6
2.1 Land use.....	6
2.2 Image and character.....	6
2.3 Access and movement	16
2.4 Sustainability and water management.....	17
2.5 Infrastructure delivery and development staging	20
3. Land budget.....	22
4. Cross-sections	23

PLANS

Plan 1 – Broader urban context	3
Plan 2 - Future Urban Structure	4
Plan 3 - Sub-precinct plan	5
Plan 4 - Built form outcomes	9
Plan 5 - Landscape and building setbacks	10
Plan 6 - Public realm and open space	15
Plan 7 - Access and movement	19

TABLES

Table 1 - Public Open Space Contribution	13
Table 2 - Land budget	22

FIGURES

Figure 1 - Green Spine indicative concept design	14
Figure 2 - Key local street cross-section	24
Figure 3 - Key local street (residential interface) cross-section	25
Figure 4 - Key local street (hospital interface) cross-section	26
Figure 5 - Green Spine cross-section	27
Figure 6 - Green Spine and local street cross-section	28

1 OUTCOMES

1.1 Vision

The WHPW will form part of a nationally competitive health precinct and will become a key hub for employment which will support health and education activity within the City of Knox. Located immediately to the west of land owned by the Department of Health, the precinct will expand on the existing health and educational institutions, and it is expected to become a sustainable place for the community and a nationally recognised health centre in Melbourne's eastern region.

The WHPW is a vibrant precinct anchored by new public and private health services for Melbourne's eastern region. Developed within a green and park-like setting, the WHPW offers a natural, nurturing, and sustainable environment that promotes health and wellbeing for patients, visitors, workers, and residents.

The WHPW will deliver a diversity of uses including health, education and complementary housing, office, and commercial uses that leverage off major public and private health facilities, providing for the everyday needs of workers and new residents.

1.2 The precinct

The WHPW is shown on Map 1. It is comprised by the land known as 706 and 760 Boronia Road, Wantirna and 750 / 750A Boronia Road, Wantirna and is subject to Schedule 3 to the Comprehensive Development Zone of the Knox Planning Scheme.

The WHPW forms part of a wider health precinct located in Wantirna within the north-western area of the City of Knox municipality, generally bounded by major roads including EastLink tollway to the west, Boronia Road to the north and Wantirna Road to the east.

1.3 Sub-precincts

WHPW has been planned for based on the following three precincts.

Sub-Precinct 1 is primarily a health, tertiary education and research sub-precinct which will leverage off its connection to adjacent health facilities on the Department of Health land to the east and provide TAFE/tertiary education and research opportunities. The sub-precinct will also include private health uses such as allied health services. This sub-precinct also includes the 'urban square'. Uses adjoining the urban square and green link in this sub-precinct will include convenience retail and food services.

Sub-Precinct 2 forms part of the northern gateway to the WHPW and will be the commercial edge which provides a buffer from Boronia Road to the residential use and development in Sub-Precinct 3 to the south. This sub-precinct will primarily comprise office and commercial buildings which accommodate a range of complementary uses to support the public health facilities, e.g., medical centres, specialist consulting suites, General Practitioner clinics, serviced accommodation and offices.

Sub-Precinct 3 is a residential sub-precinct which will provide a diverse range of housing opportunities for the new workforce and future residents. This sub-precinct will consider housing typologies that accommodate for all life stages including apartments and townhouses, key worker housing, aged care and opportunities for 10 per cent affordable housing. The sub-precinct will integrate with the retarding basin / wetlands and highlight the natural topography of the land. The sub-precinct also provides a gradual built form transition with the future residential development to the south of the WHWP and the existing Hungarian Community Centre at 760A Boronia Road, Wantirna to the north-west. Refer to Plan 2 'Future Urban Structure Plan' and Plan 3 'Sub-Precinct Plan'.

1.4 Strategic content

Plan Melbourne identifies Knox Private Hospital as a state-significant health precinct, aiming to 'support the significant employment and servicing role of health and education precincts across Melbourne'. These precincts are expected to stimulate innovation, create employment and are of fundamental importance to the emerging knowledge economy and surrounding communities. Co-location of facilities (for example, a university with a hospital) is encouraged to utilise existing infrastructure and support the growth of associated businesses and industries. Specialised economic functions with opportunities to provide

complementary retail, commercial, accommodation and supporting services are also encouraged.

The WHPW forms part of an identified employment cluster, which includes 2 sites investigated for employment growth:

- 706 and 760 Boronia Road
- 750 / 750A Boronia Road

This CDP addresses how the future use and development of the WHPW will be undertaken in a coordinated way consistent with the future direction identified in the Master Plan for the broader precinct and Knox Local Policy, leveraging the existing health infrastructure, along with adjoining land to implement key State and Local government policy objectives. Refer to Plan 1 'Broader Urban Context'.

This CDP is incorporated in the Knox Planning Scheme and addresses future land use, built form, landscaping, transport and access, drainage, and other infrastructure requirements to support the delivery of staged development of the WHPW.

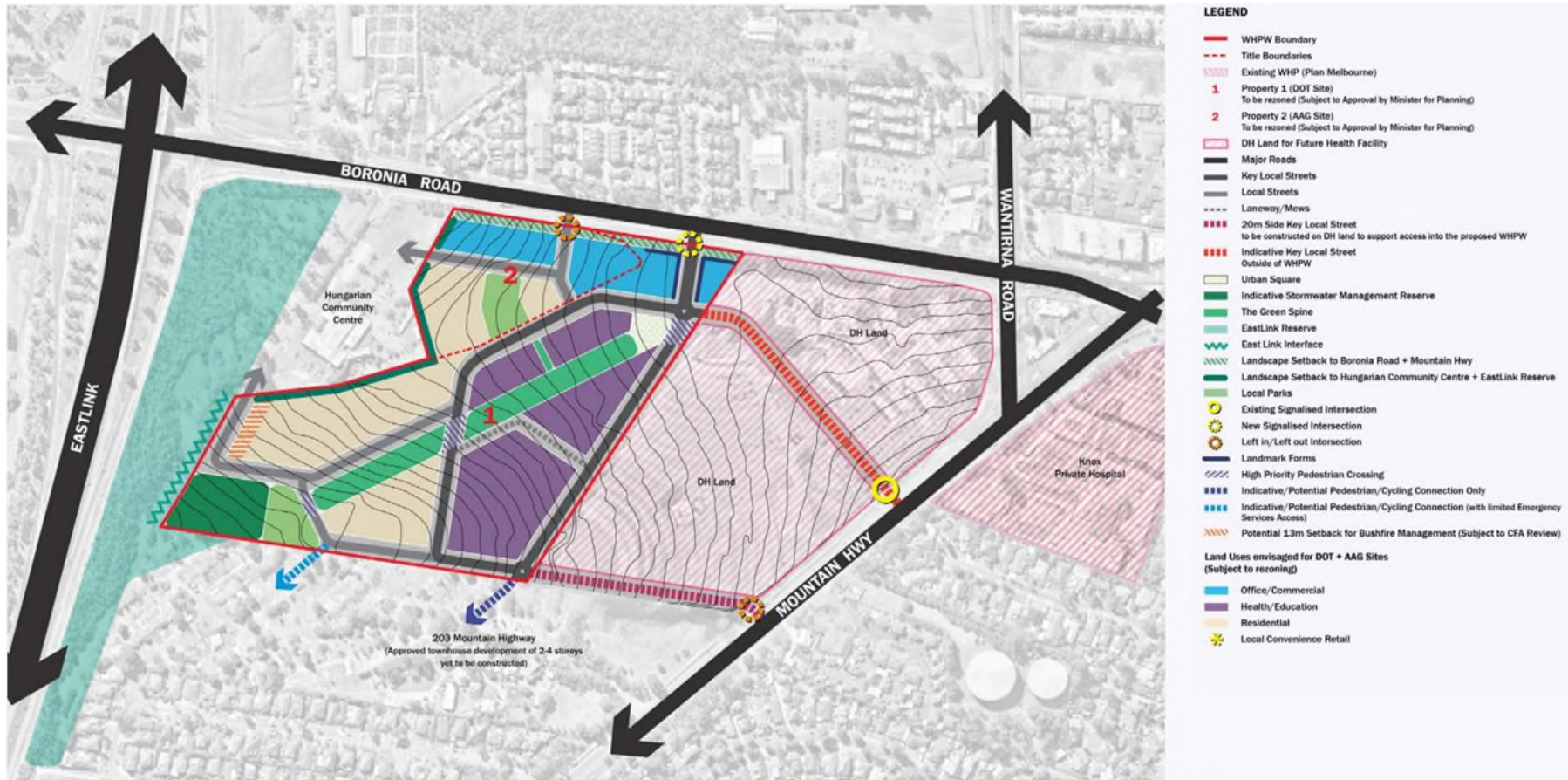
1.5 Objectives

OBJECTIVES	
LAND USE	
01	To create an employment hub which delivers uses that support health and education uses.
02	To provide for new and the expansion of existing health and educational institutions that serve a regional catchment.

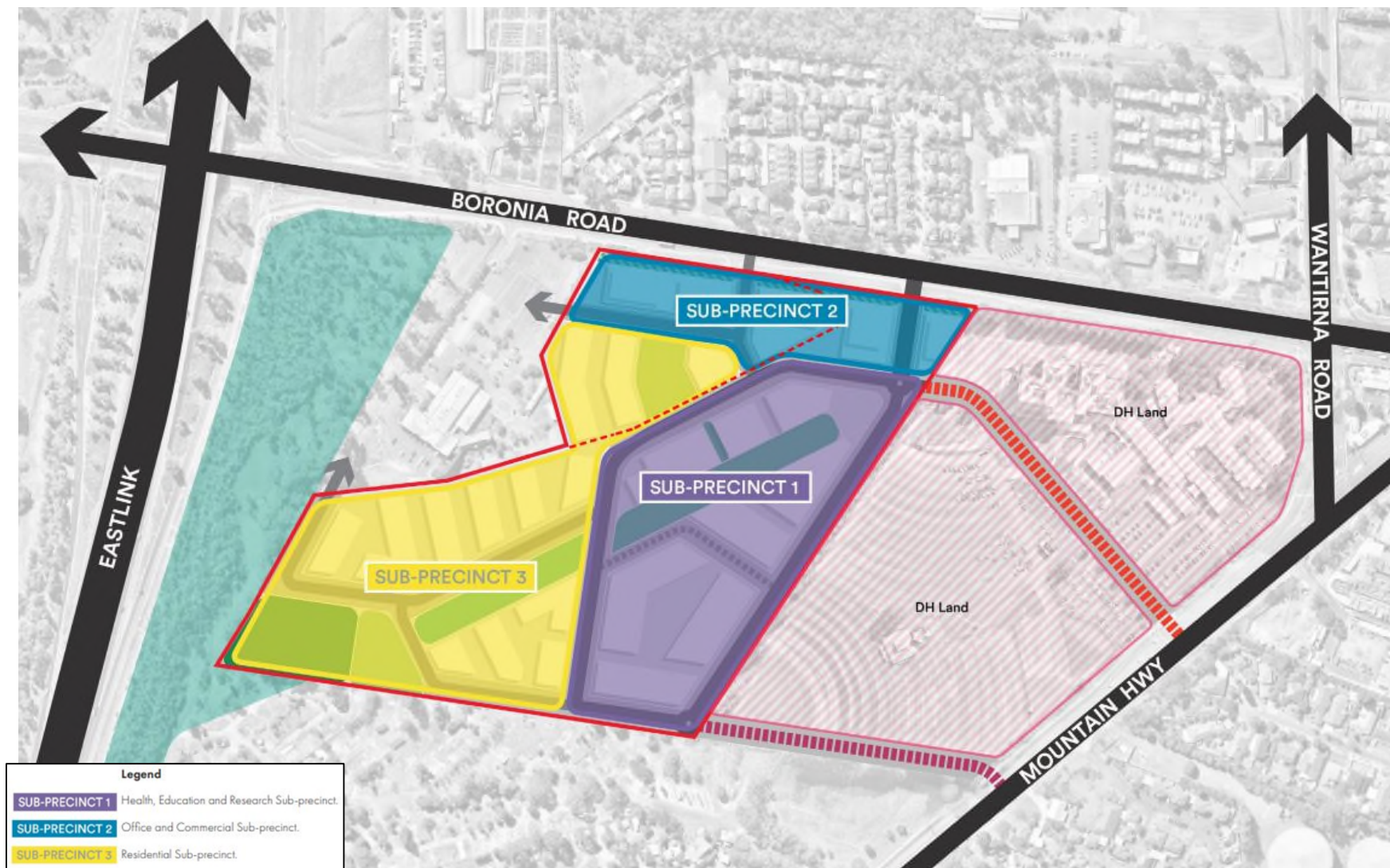
03	To encourage uses which complement and support the health and education focus of the WHPW.
04	To promote lot and dwelling types that allow for a diversity of households, including affordable housing and key worker housing.
IMAGE AND CHARACTER	
05	To deliver a vibrant urban environment, with safe streets and public spaces with a 24hr economy and a place that is active both day and night.
06	To create a strong sense of place through distinct, high-quality architecture, public realm and landscaping treatments that respond to climate change and utilise the topography of the site to create an interesting and site responsive design.
07	To ensure development is responsive to sensitive land uses such as health and residential uses within the WHPW and on adjacent land.
ACCESS AND MOVEMENT	
08	To deliver a network of streets and other public spaces, including local parks and linear open spaces, that provide high quality landscape outcomes, promote social interaction, relaxation and respite and encourage walking and cycling.
09	To establish an integrated transport network that reduces dependency on private vehicles, maximises access to public transport, encourages active transport (including walking and cycling) and sustainable transport.
SUSTAINABILITY AND WATER MANAGEMENT	
010	To create WHPW as a low carbon precinct, creating a sustainable and healthy place that reduces environmental impact through efficient use of water, energy, materials, waste and transport, and considers the future impacts of climate change by facilitating the establishment of new canopy trees, enhancing biodiversity and pursuit of urban forest principles.



Plan 1 – Broader urban context



Plan 2 - Future Urban Structure



Plan 3 - Sub-precinct plan

2 IMPLEMENTATION

2.1 Land use

OBJECTIVES	
O1	To create an employment hub which delivers uses that support health and education uses.
O2	To provide for new and the expansion of existing health and educational institutions that serve a regional catchment.
O3	To encourage uses which complement and support the health and education focus of the WHPW.
O4	To promote lot and dwelling types that allow for a diversity of households, including affordable housing and key worker housing.

2.1.1 Land use

REQUIREMENTS	
R1	All land uses, including commercial uses, must support the primary function of the WHPW as a health precinct and implement the relevant sub-precinct as described in 1.3 Sub-precincts.
R2	Applications incorporating residential use or development must demonstrate a diversity of dwelling size and typologies which may include (but is not limited to) a mix of one, two, and three-bedroom apartments and townhouses. This requirement applies to all development applications and use applications.
GUIDELINES	
G1	Retail and office land uses that encourage activation at ground level are supported in Sub-Precincts 1 and 2.
G2	Residential land uses should incorporate a range tenancy types (i.e. long-term accommodation, short-term accommodation, key-worker housing, student accommodation, retirement living, etc).
G3	Buildings located in Sub-Precincts 1 and 2 should provide a mix of commercial and retail tenancy types and sizes, encouraging fine grain tenancies fronting key streets and public realm.

G4	Land uses with extended operating hours are encouraged within Sub-Precincts 1 and 2 to facilitate activation at different times of the day and evening.
G5	Convenience retail should be located near the Urban Square.
G6	Mixed-use buildings should provide retail or office uses on the lower levels with residential limited to the upper levels.

2.2 Image and character

OBJECTIVES	
O5	To deliver a vibrant urban environment, with safe streets and public spaces with a 24hr economy and a place that is active both day and night.
O6	To create a strong sense of place through distinct, high-quality architecture, public realm and landscaping treatments that respond to climate change and utilise the topography of the site to create an interesting and site responsive design.
O7	To ensure development is responsive to sensitive land uses such as health and residential uses within the WHPW and on adjacent land.

2.2.1 Built form, building design and siting

REQUIREMENTS	
R3	<p>Development must not exceed the maximum height specified in Plan 4 unless the proposal delivers an excellent design outcome that achieves the objectives of the CDP and avoids unreasonable off-site impacts as a result of additional height. This will require consideration of:</p> <ul style="list-style-type: none"> - Whether the proposal achieves design excellence; - Whether the proposal contributes to a diversity of building heights, with the tallest heights adjacent to gateways and main roads; - Whether the proposal contributes to variations in building height and form across the site to effectively manage visual bulk and allow for visual interest; - Whether the proposal makes an exceptional contribution to the quality of the public realm; and - The provision of positive design features such as green roofs and green walls.

R4	Loading, storage and building services including domestic services, utilities and waste management facilities must be concealed and integrated into building design so as not to be visible from public areas.
R5	In Sub-Precincts 1 and 3, buildings adjacent to the Green Spine must be sited and designed to positively address the open space reserve through the siting of clear windows, balconies and legible pedestrian entry points. Pedestrian entry points must be designed to avoid conflicts with users of the Green Spine and to maximise the safety of pedestrians and cyclists.
R6	Office and commercial buildings must have a minimum floor to floor height of 4.5 metres on ground floor and 3.5 metres above, to allow buildings to be adapted in the future.
R7	Podiums must have a maximum height of 4 storeys. Development above podium height must be setback from the building edge below by a minimum of 5 metres.
R8	Buildings must be located outside the defendable space on Plan 3 – Sub-Precinct Plan and comply with applicable BAL ratings.
R9	In Sub-Precinct 3, buildings adjacent to the Eastlink Interface must be designed and sited in a manner which mitigates traffic noise.
R10	Residential buildings greater than 4 storeys must be spaced to equitably distribute access to sunlight, maintain residential privacy, maximise opportunities to a view or outlook between buildings, ensure adequate sun penetration at street level and allow for tree planting.
R11	<p>New development must:</p> <ul style="list-style-type: none"> - not unreasonably reduce the amenity of public spaces by casting excessive shadows on any public space, including public parks and gardens, the Green Spine, major pedestrian routes including streets and lanes and privately-owned spaces accessible to the public; - be designed to avoid casting unreasonable shadows that would materially detract from the amenity of any public space, including public parks and gardens, the Green Spine, major pedestrian routes including streets and lanes, between 11:00am and 2:00pm on 22 September; and - not cast additional overshadowing to public open space, including but not limited to the Green Spine, beyond the shadow that would have otherwise occurred under the preferred maximum heights and minimum setbacks specified in Plan 4 and Plan 5 where the maximum height in plan 4 has been exceeded.

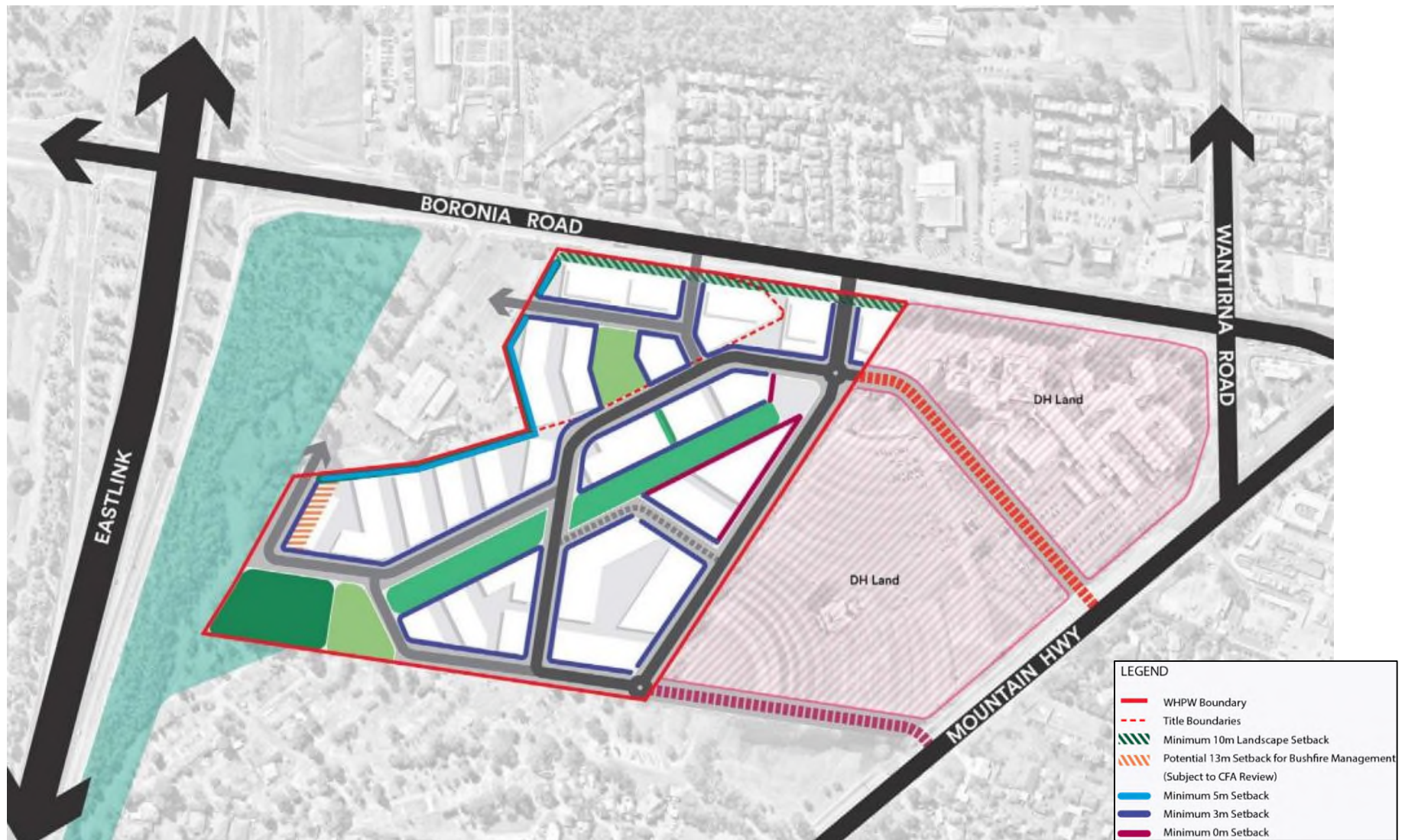
R12	All buildings must demonstrate high quality, visually attractive and site responsive design that incorporates high-quality materials (i.e. robust, durable, sustainable and visually attractive) and finishes.
R13	Key Built Form sites identified in Plan 4 must be designed with built form elements that strengthen their importance and location, (including through high-quality building facades made from robust, durable, sustainable materials that complement surrounding buildings and provide visual interest).
R14	All buildings must provide dedicated car charging stations unless otherwise agreed by the Responsible Authority.
R15	All buildings greater than 4 storeys must provide dedicated parking for car share unless otherwise agreed by the Responsible Authority.
GUIDELINES	
G7	Buildings should be setback in accordance with Plan 5 and accord with the relevant cross sections outlined in Part 4 – Cross-sections.
G8	Setbacks above podiums should be emphasised through a change in architectural expression, material selection and design elements.
G9	<p>All buildings should provide frontages at street level in accordance with Plan 5 and respond to the relevant description below:</p> <p>High Priority Active Frontages: To include a high level of visual permeability, particularly on the ground floor with 80% clear glazing, fine grain elements that are made up of closely placed tenancies with smaller floor plates, high quality architectural detailing, opportunities for informal seating and outdoor dining within building setbacks.</p> <p>Active Frontages: To include a high level of visual permeability, particularly on the ground floor with 80% clear glazing, with flexibility to allow a slightly lower level of visual permeability and activation to allow for privacy, where required.</p> <p>Semi-Active Frontages: To provide a high-quality architectural response through articulation and detailing, fine grain elements and allow to include a lower level of visual permeability, where required.</p> <p>Residential Active Frontage: Should include a high-quality architectural response and provide a sense of address with windows and direct individual street entries to dwellings, while limiting the visual impact of garages and crossovers.</p>
G10	Tower forms should be slender to reduce the perception of visual bulk when viewed from streets.

G11	<p>All buildings should incorporate features of visual interest, such as:</p> <ul style="list-style-type: none"> - Parapets, awnings, shade structures, roof elements; - Sculptured facades of recesses and projections to provide variation and depth; - Vertical elements that provide segmentation; - Balconies and verandahs that define building floors or storeys; - Façade articulation; and - Features materials and colours.
G12	The number of vehicle crossovers along the key local streets shown on plan 7 should be minimised.
G13	Buildings should be designed and sited to provide sightlines to the Stormwater Management Reserve from adjoining properties/development within the precinct.
G14	Dwellings should be sited and constructed to BAL 12.5 in bushfire prone areas.
G15	In Sub-Precinct 2, buildings should address the major north-south road through the WHPW and include appropriate fenestration on all levels to provide surveillance and safety to the street.
G16	In Sub-Precinct 3, buildings opposite the Stormwater Management Reserve should be oriented to provide views across the wetland / open space area as well as towards the EastLink reserve and limit potential overshadowing of the Stormwater Management Reserve.
G17	All elements of the façade, including balconies, and roof areas should be integrated into the architectural form and detail of the building and present an appealing streetscape appearance.
G18	Buildings on corner sites should be designed to address both street frontages, through the use of fenestration, architectural elements, awnings, pedestrian entries and active uses.

G19	<p>In Sub-Precinct 1 and 3, where dwellings with garages are proposed, the visual impact of garages should be minimised through:</p> <ul style="list-style-type: none"> - Locating garages to the side or rear of the development, accessed by laneways, where appropriate having regard to the ability to ensure vehicle delivery access and acceptable levels of passive surveillance of rear access lanes. - Integrating garages into the built form by setting them back behind the front wall and including upper storey built form above with balconies / windows. - Garages should be setback 5.5m to prevent vehicles overhanging the footpath.
G20	<p>The design and layout of car parking areas should:</p> <ul style="list-style-type: none"> - Allow natural ventilation, without compromising the provision of activated frontages. - Include provision for future conversion of car parking areas to alternative employment generating uses. - Ensure the use of car lifts, turntables and stackers do not result in cars queueing on the street. - Include the provision for internal queuing and minimise the need for cars to queue on the street. - Ensure the layout and design of car parking areas encourages sharing of car parking spaces between different uses with different peak demand patterns and smart parking management systems.
G21	Carparking should be provided at the basement level or semi-basement, where topography allows.
G22	Long extents of buildings greater than 10 metres should be broken up by using a combination of varied setbacks, articulation, materials and colours, as appropriate, to contribute to variation and architectural interest.
G23	Any building with an aggregate length exceeding 55 metres should provide a through site link to enable pedestrian connectivity.
G24	Development should avoid dark materials that absorb heat and contribute to urban heat island effect.
G25	Fences should not be provided at the front of development. Where fences are proposed at the front of properties, they should not exceed 1.2 metres.



Plan 4 - Built form outcomes



Plan 5 - Landscape and building setbacks

2.2.2 Landscape and trees

REQUIREMENTS	
R16	Street trees must be planted on both sides of all new roads and streets at regular intervals (appropriate to tree size at maturity), unless otherwise agreed by the Responsible Authority, using species to reduce bushfire risk.
R17	In accordance with Plan 5, landscape setbacks must be provided as follows: <ul style="list-style-type: none"> - A minimum 10 metre landscape setbacks along Boronia Road; and - A minimum 5 metre landscape setback along the boundary interface with the adjacent property at 760A Boronia Road, Wantirna (existing Hungarian Community Centre).
R18	All trees in public areas must be passively watered by stormwater runoff.
R19	Existing high retention value trees where they have a positive contribution to the character of the area should be retained and protected. Where possible, exotic trees should be replaced with Australian native/indigenous trees to the satisfaction of the Responsible Authority.
GUIDELINES	
G26	Consistent public lighting, furniture, informational and way-finding signage should be used across the WHPW, within all public areas including the public plaza, along all major streets, shared, pedestrian and cycle paths.
G27	Trees in public open spaces, landscape setbacks and streets should be: <ul style="list-style-type: none"> - Larger species wherever space allows (to facilitate continuous canopy cover); - Appropriate in size to nature strips, nearby utilities and built form; and - Inclusive of species with selection favouring local provenance trees, indigenous native species, and drought tolerant planting consistent with any guidance provided on the relevant cross section in Part 4.

G28	Tree and vegetation planting in landscape setbacks along the boundary interface with the adjacent property at 760A Boronia Road, Wantirna (Hungarian Community Centre) should provide: <ul style="list-style-type: none"> - Larger Species wherever space allows (to facilitate continuous canopy cover), - Species appropriate in size to the landscape setback and built form, to reduce the appearance of visual bulk; and - A variety of species with the selection favouring local provenance trees, indigenous native species, and drought tolerant planting. 								
G29	Street trees should be planted at the following average intervals and heights, using species to reduce bushfire risk: <table border="1"> <thead> <tr> <th>Average interval</th><th>Tree size (in height) at maturity</th></tr> </thead> <tbody> <tr> <td>5–7 metres</td><td>Small trees (less than 10 metres)</td></tr> <tr> <td>7–10 metres</td><td>Medium trees (10–15 metres)</td></tr> <tr> <td>10–15 metres</td><td>Large trees (15 metres or greater)</td></tr> </tbody> </table> <p>Tree species should be selected from the <i>Knox Street Tree Asset Management Plan (2016)</i>.</p>	Average interval	Tree size (in height) at maturity	5–7 metres	Small trees (less than 10 metres)	7–10 metres	Medium trees (10–15 metres)	10–15 metres	Large trees (15 metres or greater)
Average interval	Tree size (in height) at maturity								
5–7 metres	Small trees (less than 10 metres)								
7–10 metres	Medium trees (10–15 metres)								
10–15 metres	Large trees (15 metres or greater)								
G30	Podium rooftops should provide communal open space and landscaping, maximising north orientation and providing partial cover from weather, where feasible.								

2.2.3 Topography

REQUIREMENTS	
R20	Subdivision and development applications for land with a slope greater than 10% must respond to the key design principles indicated on Figure 1 – Green Spine indicative concept design.
R21	The design of the Green Spine shown on Plan 2 must respond sensitively to the natural topography and must be provided generally in accordance with Plan 2 to the satisfaction of the Responsible Authority.
R22	The Green Spine must be fully accessible for people with reduced mobility, and have a maximum slop gradient of 5%.

GUIDELINES	
G31	Built form on sloping sites should be designed to integrate with the natural topography to have minimal intervention to the natural grade. This may be achieved by designs that are responsive to the gradient of the land such as the use of split-level designs, undercroft structures and the considered built form volumes.
G32	The Green Spine should be designed to manage slope and maximise soft landscaping potential.
G33	The Green Spine should: <ul style="list-style-type: none"> – Provide level entry access for pedestrians. – Be designed to avoid excessive use of ramps, switchbacks and hardstand areas, where possible. – Provide path connections to the entries of buildings fronting the Green Spine.
G34	Any retaining structures within public and private spaces (except for those which are part of a building) should be: <ul style="list-style-type: none"> – no more than 1.0 metre in height between a dwelling and a street or public space, or where visible from a street or public space – staggered, with a minimum 0.75 metre distance between each stagger to allow for the inclusion of landscaping where cutting and filling is deeper than 1.0 metres – positioned so that associated drainage infrastructure and structural foundation are fully located within the same lot.

2.2.4 Open space

REQUIREMENTS	
R23	Not less than the amount of unencumbered public open space specified on Table 1 (in hectares) must be provided generally in accordance with Plan 6 to the satisfaction of the Responsible Authority.
R24	The Stormwater Management Reserve in Sub-Precinct 3 must include a pedestrian or cycling connection to the adjoining EastLink Reserve and must not preclude a future shared path which enables a connection to Dandenong Creek.
R25	Fencing adjoining open space must be low in scale and visually permeable.

R26	The Green Spine must acknowledge indigenous and non-indigenous themes of health and well-being, culture and heritage within the landscaping outcomes and in consultation with the Wurrundjeri Woi Wurrung.
GUIDELINES	
G35	Public open space should comprise a variety of spaces to support a broad range of activities responsive to the needs of residents, workers and visitors at different times of the day and week, including: <ul style="list-style-type: none"> – Flexible spaces that support a range of community activities including passive and active recreation; – Tree plantings and landscaping elements for shading and cooling; – Water management elements; – Native plant species that have lower fire risks and are drought tolerant; and – Light coloured pavements and road surfaces to avoid heat absorption and retention.
G36	Public open space network should comprise a series of connected spaces which facilitate ease of movement for both pedestrians and cyclists within the site and to or from the surrounding area.
G37	The design of public open space should enhance public safety through lighting, open sightlines and landscaping that allow high levels of public surveillance in accordance with Crime Prevention through Environmental Design principles (CPTED) which are incorporated in the Urban Design Guidelines for Victoria.
G38	The urban square should be located adjacent to the main entry of WHPW from Boronia Road, adjoining the Green Spine to provide an accessible gathering place. It should provide a hardscaped public forecourt to the future retail/education/health buildings within Sub-Precinct 1 and maintain a strong connection to the adjacent Hospital and its public spaces.
G39	The Green Spine should have a lush, green character with grassed areas, trees, shared paths, and opportunities for seating and exercise/play equipment. It should provide a place for relaxation and respite for the surrounding workers, visitors and residents with consideration given to appropriate lighting and passive surveillance at darker hours of the day. Where possible lighting should minimise impact on wildlife.

G40	The Local Parks should contain large grassed areas, significant trees and landscaping, and opportunities for passive recreation / play areas.
G41	Where appropriate existing vegetation and significant trees should form an integrated part of new public open space.

Table 1 - Public Open Space Contribution

Unencumbered Open Space	Property	Type	Hectares	Per cent NDA
Urban Square	706 and 760 Boronia Road	Urban Plaza	0.11	0.98%
The Green Spine (total of two sections)	706 and 760 Boronia Road	Green spine	0.88	7.87%
Local Residential Park 1	706 and 760 Boronia Road	Local Park	0.26	2.32%
Local Residential Park 2	750 & 750A Boronia Road	Local Park	0.24	2.14%
Total Public Open Space Contribution			1.49	13.31%
Encumbered Open Space	Property	Type	Hectares	Per cent NDA
Stormwater Management Reserve	706 and 760 Boronia Road	Drainage	0.54	N/A

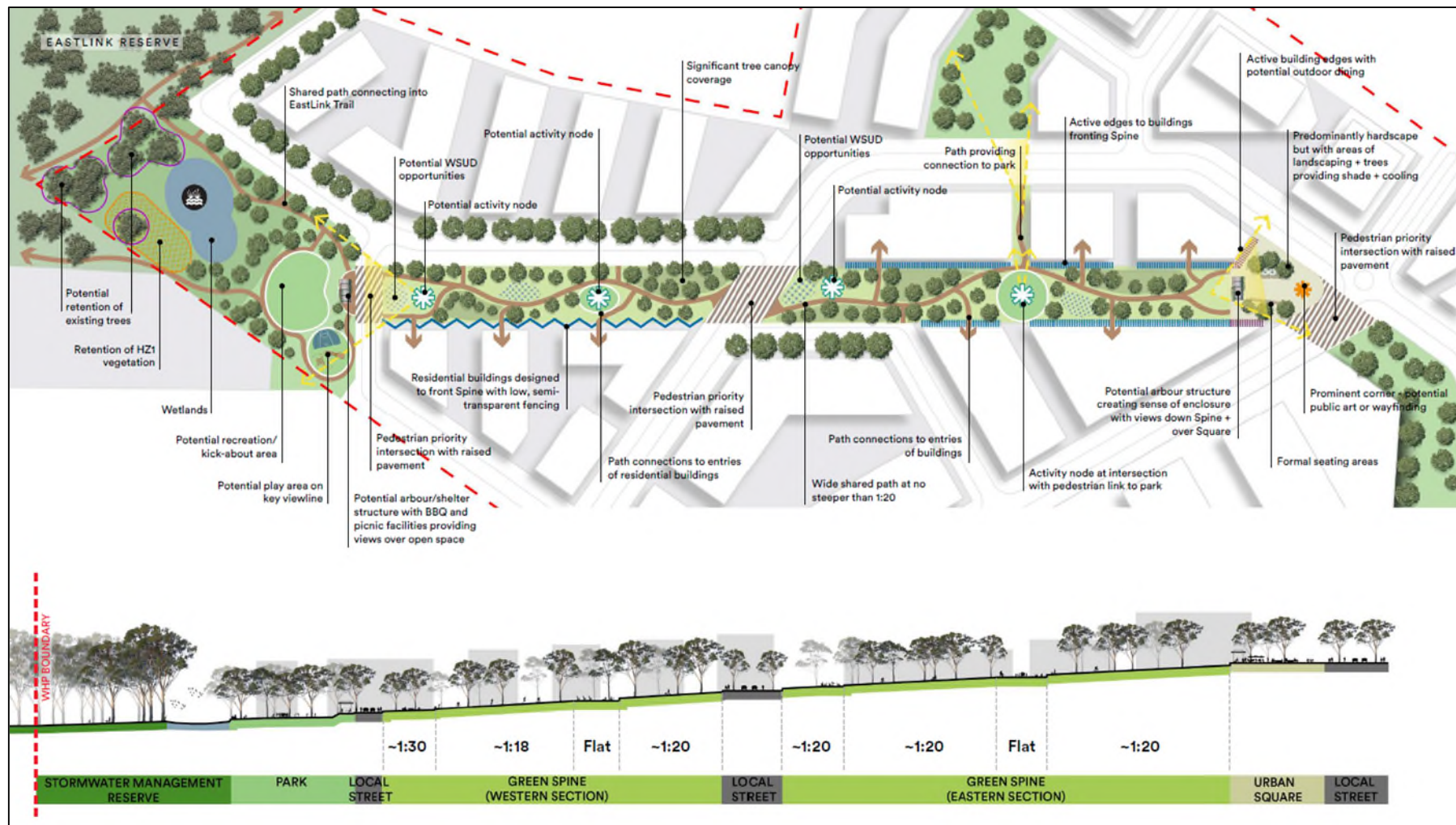
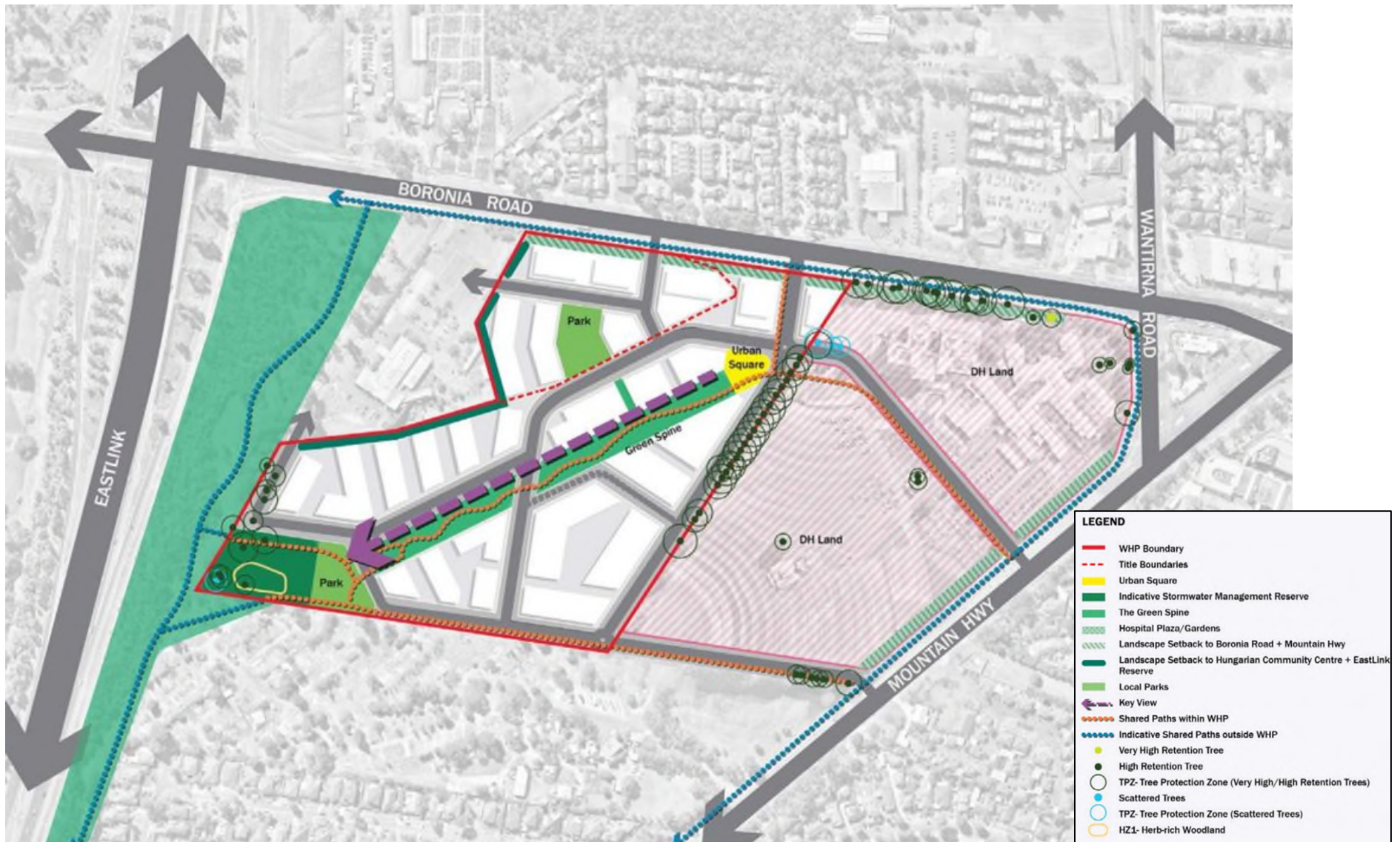


Figure 1 - Green Spine indicative concept design



Plan 6 - Public realm and open space

2.3 Access and movement

OBJECTIVES	
O8	To deliver a network of streets and other public spaces, including local parks and linear open spaces, that provide high quality landscape outcomes, promote social inclusion, relaxation and respite and encourage walking and cycling.
O9	To establish an integrated transport network that reduces dependency on private vehicles, maximises access to public transport, encourages active transport (including walking and cycling) and sustainable transport.

2.3.1 Streets

REQUIREMENTS	
R27	Vehicle access to the WHPW from Boronia Road must only be provided via the signalised intersection and via the left-in, left-out access point, as shown on Plan 7.
R28	Future road connections must be provided to the adjoining land to the west at 760A Boronia Road Wantirna (existing Hungarian Community Centre).
R29	Future pedestrian and cycle access must be provided to the adjoining land to the west at 760A Boronia Road Wantirna (existing Hungarian Community Centre).
R30	The internal accessways and traffic calming measures must be designed and constructed with a maximum speed of 30 km/hr to create a safe and pedestrian friendly environment.
R31	Roads, including traffic calming measures, must be designed to facilitate the access and operation of emergency service vehicles to the satisfaction of the Responsible Authority.
R32	Design of streets, pedestrian links and open spaces must actively encourage sustainable transport usage including cycling, walking, public transport.
R33	All land in the WHPW must be provided with convenient and direct access to the road network to the satisfaction of the Responsible Authority.

GUIDELINES	
G42	The road intersections to Mountain Highway and Boronia Road and internal road network should be designed in accordance with Plan 7 and the street cross sections shown in Section 4, to the satisfaction of the Responsible Authority.
G43	Access to common parking areas should be provided from rear or side of the lots to prioritise pedestrian movement on key local streets.

2.3.2 Walking and cycling

REQUIREMENTS	
R34	The design of the internal transport network must provide for sustainable transport opportunities.
R35	The subdivision and development of the WHPW must address all design standards provided in the Austroads Guide to Traffic Management and comply with the requirements of the Knox Planning Scheme to the satisfaction of the Responsible Authority
R36	Streets and roads (as shown on Plan 7) must be designed to give priority to the requirements of pedestrians and cyclists to the satisfaction of the Responsible Authority by providing: <ul style="list-style-type: none"> – pedestrian paths of at least 1.8 metres in width on both sides of all internal streets; – shared paths or bicycle paths of minimum 3.0 metres in width; – pedestrian and cyclist crossing points at all intersections, key desire lines and key trip-generating locations, which are safe and convenient; – pedestrian priority where local roads intersect with connector roads; – pedestrian and cyclist priority crossings on all slip lanes; – clear, consistent and visible signage, road markings and visual clues which identify priority cycle routes; – transitions between on-road and segregated bicycle connections which are safe and convenient;

	<ul style="list-style-type: none"> – consistent line and lane marking, visual clues and signage identifying cycle priority routes. <p>The designs must align with current and proposed pedestrian and cycling linkages in the surrounding area and key destinations and meet the requirements of the relevant road authority and the Responsible Authority.</p>
R37	Pedestrian priority must be achieved at Key Pedestrian Crossings as shown on Plan 7.
GUIDELINES	
G44	Pedestrian priority should be provided across all side roads along main streets and all car park entrances.
G45	Pedestrian movements should be prioritised by providing clear links between key destinations and open space within the WHPW.
G46	<p>All streets should provide:</p> <ul style="list-style-type: none"> – Safe and convenient pedestrian and cycle crossing points of connector and local streets at all intersections and at key desire lines and locations of high amenity. – Pedestrian priority where local roads intersect with connector roads and across all car park entrances. – Consistent line/lane marking, visual cues and signage identifying cycle priority routes.

2.3.3 Public transport

REQUIREMENTS	
R38	Bus stop hard stands must be constructed with direct and safe access to a pedestrian path on or adjacent to the land, at locations approved in writing by the Department of Transport and Planning.

2.4 Sustainability and water management

OBJECTIVES	
O10	To create WHPW as a low carbon precinct, creating a sustainable and healthy place that reduces environmental impact through efficient use of water, energy, materials, waste and transport, and considers the future impacts of climate change by facilitating the establishment of new canopy trees, enhancing biodiversity and pursuit of urban forest principles.

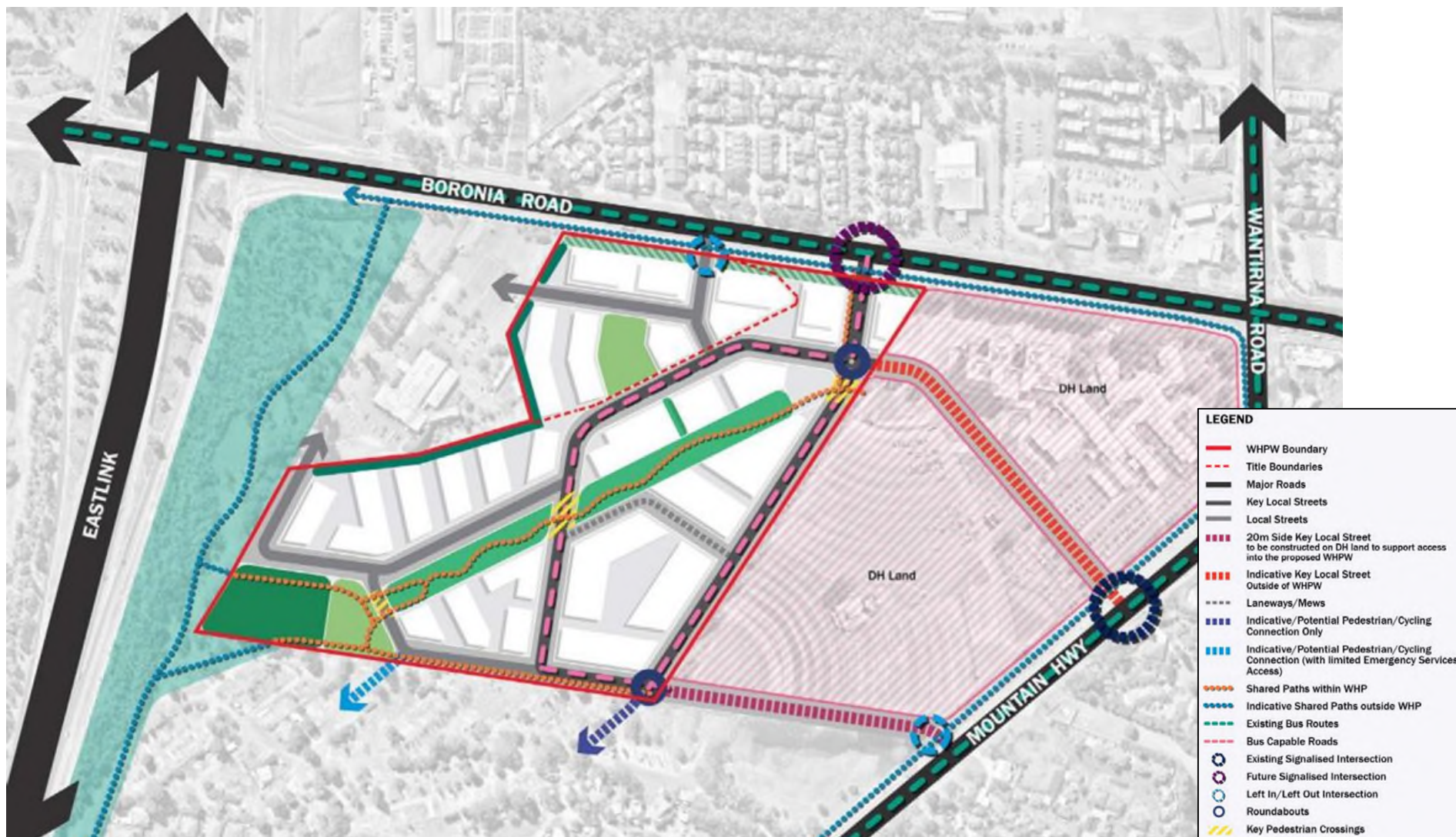
2.4.1 Integrated water management

REQUIREMENTS	
R39	<p>Unless otherwise approved by Melbourne Water and the Responsible Authority:</p> <ul style="list-style-type: none"> (a) Development must comply with the requirements of both clause 53.18 of the planning scheme and the approved Stormwater and Integrated Water Management Plan (SIWMP); and (b) stormwater runoff from new development must meet or exceed the performance objectives of the CSIRO <i>Best Practice Environmental Management Guidelines for Urban Stormwater</i> (1999) prior to discharge to receiving waterways, unless otherwise approved by Melbourne Water and the Responsible Authority.
R40	Peak flow rate of stormwater runoff from development must not exceed peak flow rate generated from the pre-development site to the satisfaction of the Responsible Authority.
R41	The design of drainage infrastructure for the purpose of conveyance, detention and stormwater quality treatment, must be to the satisfaction of the Responsible Authority.
GUIDELINES	
G47	The design and layout of roads, road reserves, and public open space should optimise water use efficiency and long-term viability of vegetation and public uses through the use of overland flow paths, Water Sensitive Urban Design initiatives such as rain gardens and/or locally treated storm water for irrigation, where practical.

G48	Developments should include Integrated Water Management systems to diversify water supply, reduce reliance on potable water and increase the utilisation of stormwater that contributes to a sustainable and green urban environment (such as stormwater harvesting, aquifer storage and recharge, grey water recycling etc) should be incorporated.
G49	Developments should encourage the reuse of water for toilets, laundries and gardens, where appropriate, and in line with the approved Stormwater and Integrated Water Management Plan (SIWMP).

2.4.2 Sustainability

GUIDELINES	
G50	Development should achieve a minimum 7-star rating under the 'Green Star – Design and As Built' rating tool, or equivalent, to the satisfaction of the Responsible Authority.
G51	Development should provide and showcase net zero emission buildings and housing throughout the precinct.
G52	Roofs of buildings should be used for: <ul style="list-style-type: none"> - Solar and wind power generation; - Solar water heating; - Roof gardens, decks and terraces.
G53	Where practicable, roads, paths and play equipment should utilise recycled material.
G54	Waste Management Plans should be prepared to implement a four-bin waste management system.



Plan 7 - Access and movement

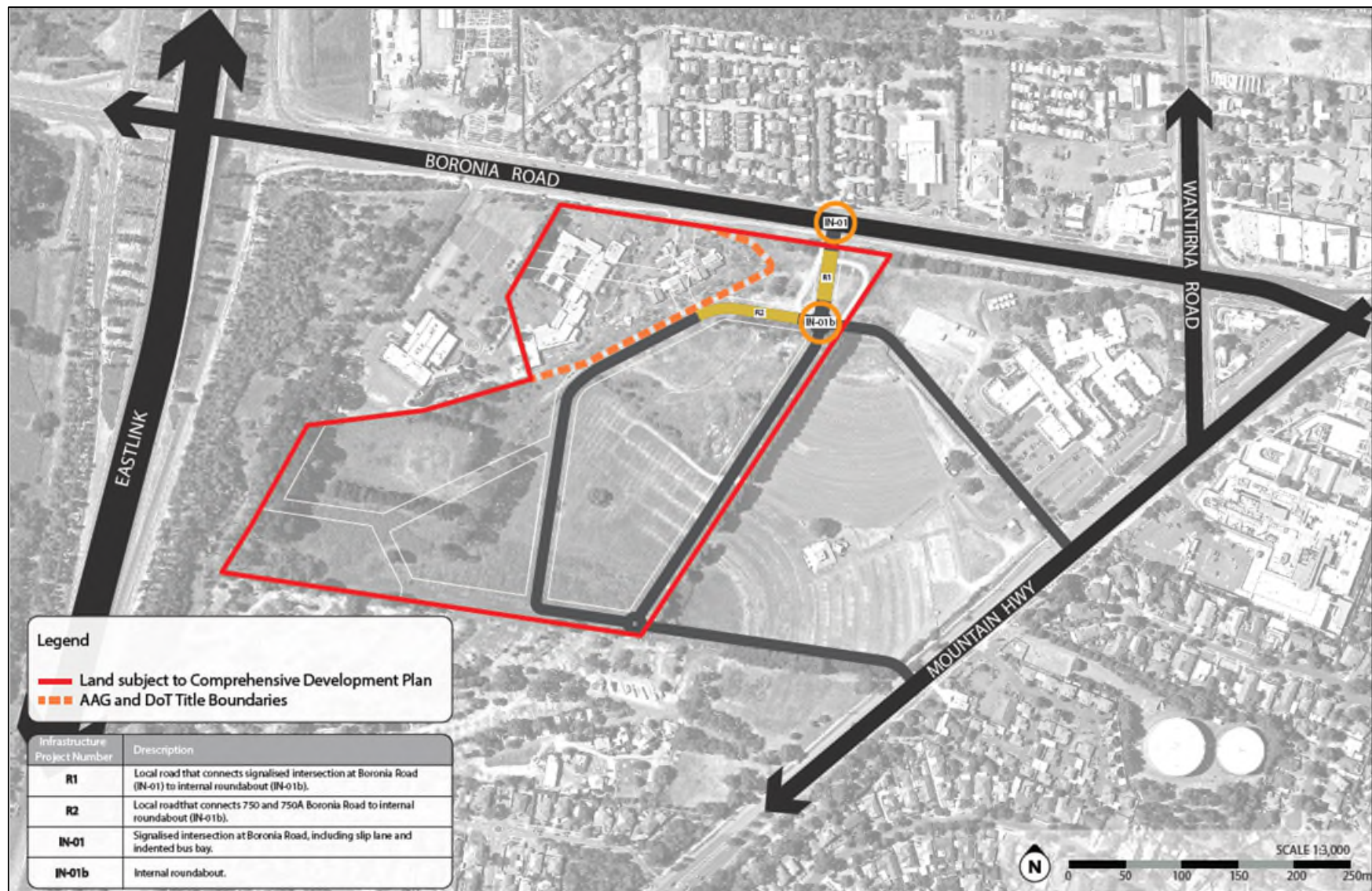
2.5 Infrastructure delivery and development staging

2.5.1 Infrastructure delivery

REQUIREMENTS	
R42	Where a street has already been constructed or approved for construction to a property boundary, subsequent development must connect with that street to adopt a consistent cross-section until a suitable transition can be made.
R43	The shared road infrastructure items shown on Plan 8 must be provided in a coordinated and timely manner and be vested in the relevant road authority.
R44	All road infrastructure must be constructed in accordance with the corresponding street and road cross sections shown in Section 4, to the satisfaction of the responsible authority.
R45	All road infrastructure must be vested in the relevant road authority as a road reserve upon the subdivision of the land.
GUIDELINES	
G55	Public open space should be delivered early to service the first occupiers of WHPW at the responsibility of the developer and vested in Council.
G56	Streetscape landscaping adjacent to development should be provided prior to occupancy of each building, unless otherwise agreed by the Responsible Authority. Coordination of landscape delivery is encouraged.

2.5.2 Development staging

REQUIREMENTS	
R40	Development staging must provide for the timely delivery of: <ul style="list-style-type: none">– Connector streets;– Street links between properties, constructed to the property boundary;– Public land areas, including open space reserves;– Drainage outfall; and– Connection of the on- and off-road pedestrian and bicycle network and public and community transport network.



Plan 8 – Shared infrastructure

3 LAND BUDGET

Table 2 - Land budget

Description	Wantirna Health Precinct West 706 and 760 Boronia Road & 750 /750A Boronia Road, Wantirna		
	HECTARES	% OF TOTAL	% OF NDA
TOTAL PRECINCT AREA (ha)	11.72	100%	
Open Space			
Uncredited Open Space			
Waterway and Drainage Reserve	0.54	4.607%	
Sub-total Uncredited Open Space	0.54	4.607%	
Credited Open Space			
Green Spine, Urban Square and Local Parks	1.49	12.71%	13.32%
Sub-total Credited Open Space	1.49	12.71%	13.32%
Total All Open Space	2.05	17.49%	
Developable Area			
Developable Area - Residential			
Residential	4.11	35.06%	36.76%
Sub-total Residential	4.11	35.06%	36.76%
Developable Area - Employment			
Health and Education	3.74	31.91%	33.45%
Commercial	1.83	15.61%	16.36%
Sub-total Employment	5.57	47.52%	49.82%
Total All Developable Area	9.68	82.58%	86.58%
TOTAL NET DEVELOPABLE AREA - (NDA) Ha	11.18	95.39%	100%

4 CROSS-SECTIONS

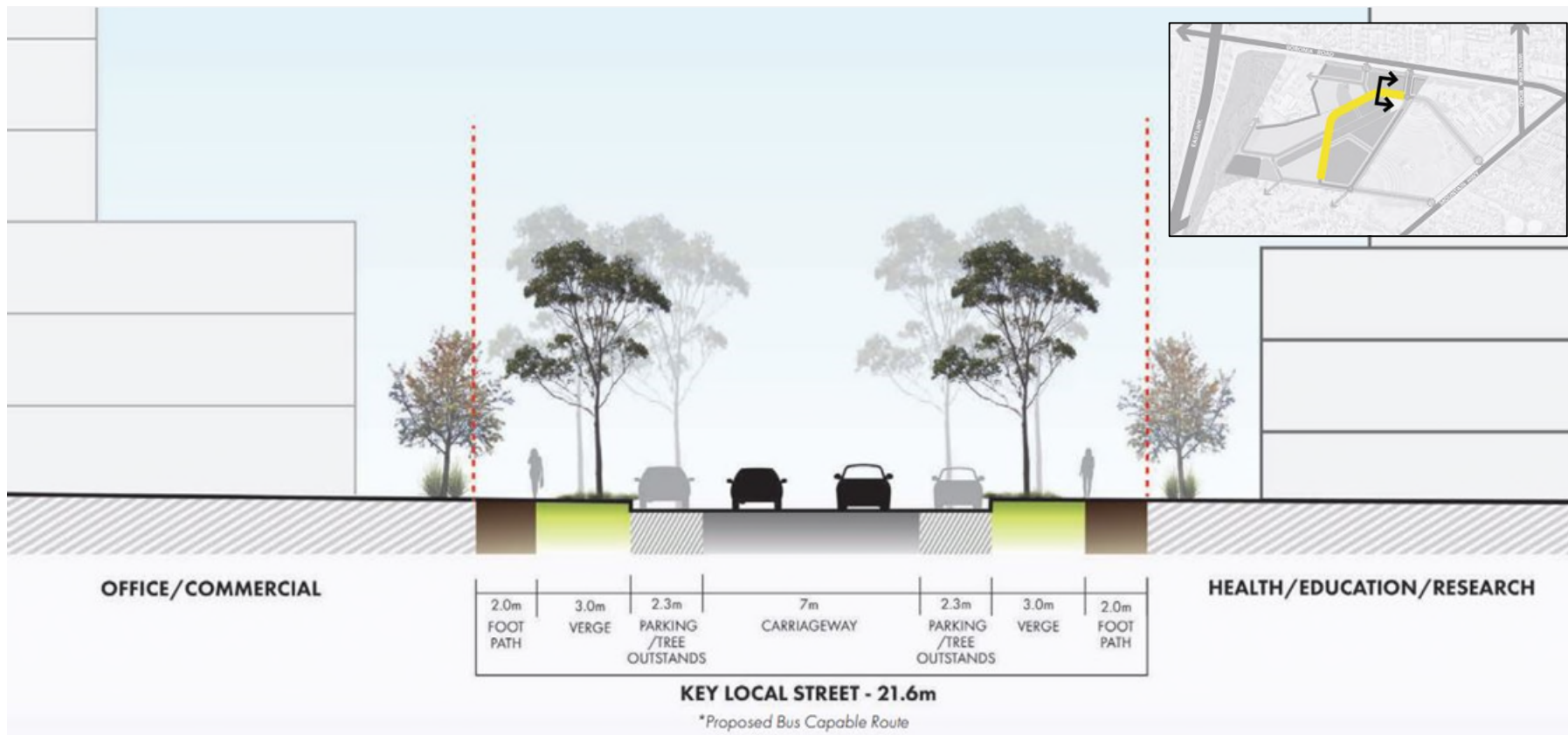


Figure 2 - Key local street cross-section

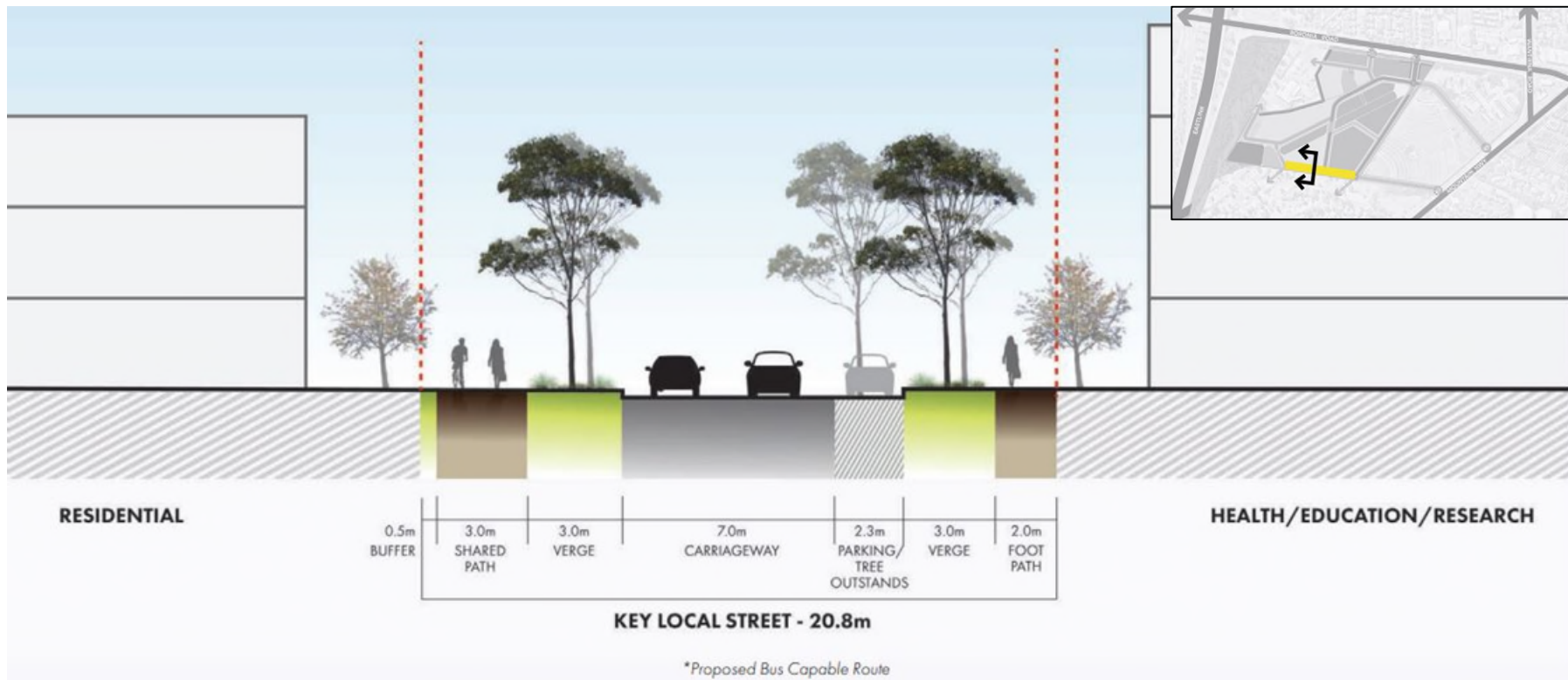


Figure 3 - Key local street (residential interface) cross-section

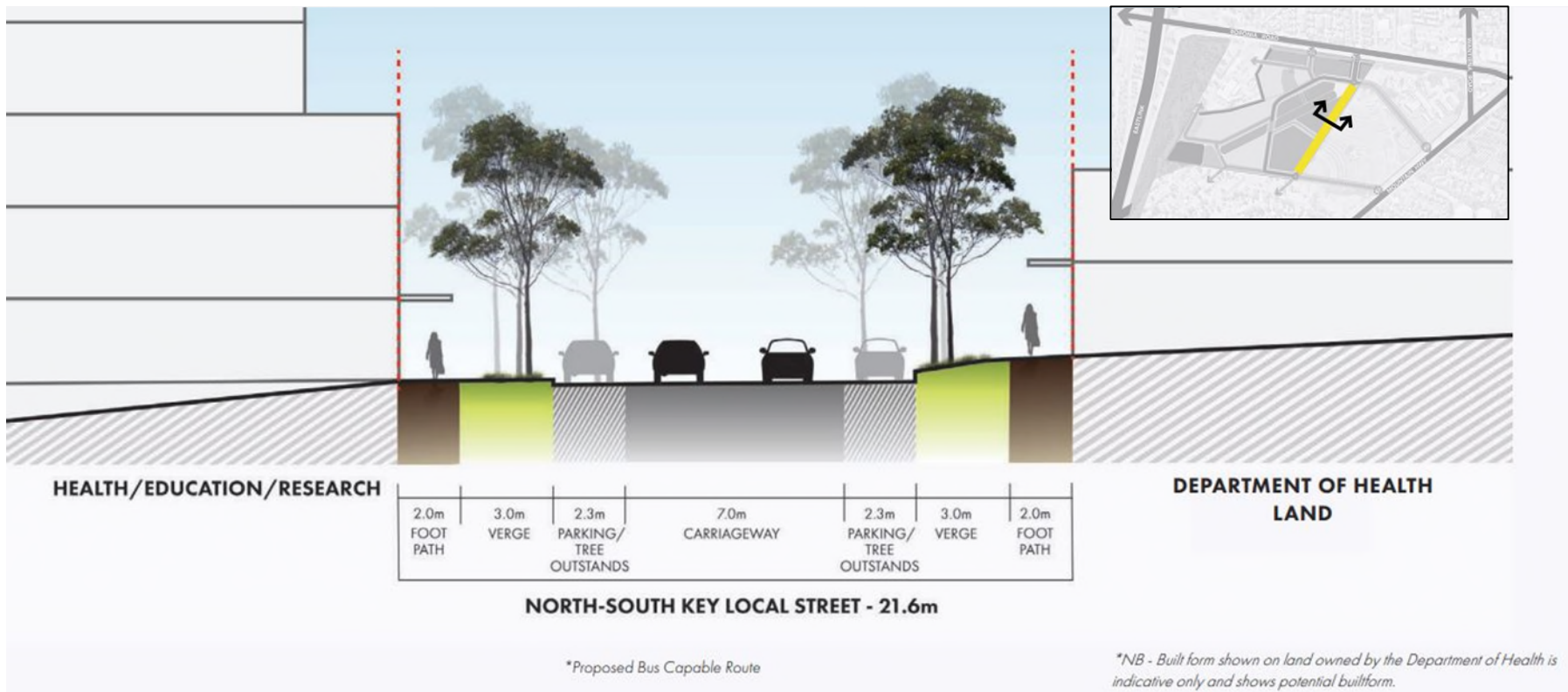


Figure 4 - Key local street (hospital interface) cross-section

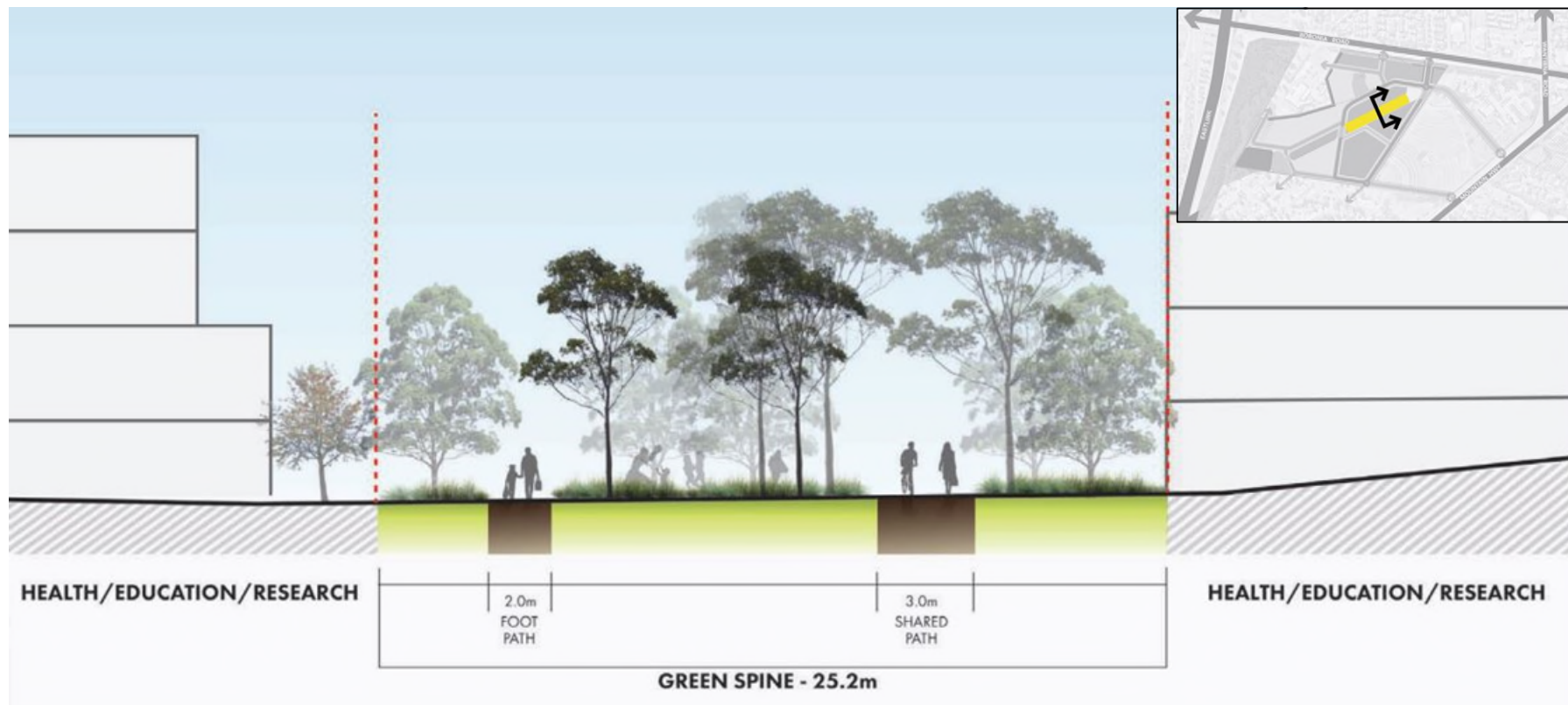


Figure 5 - Green Spine cross-section

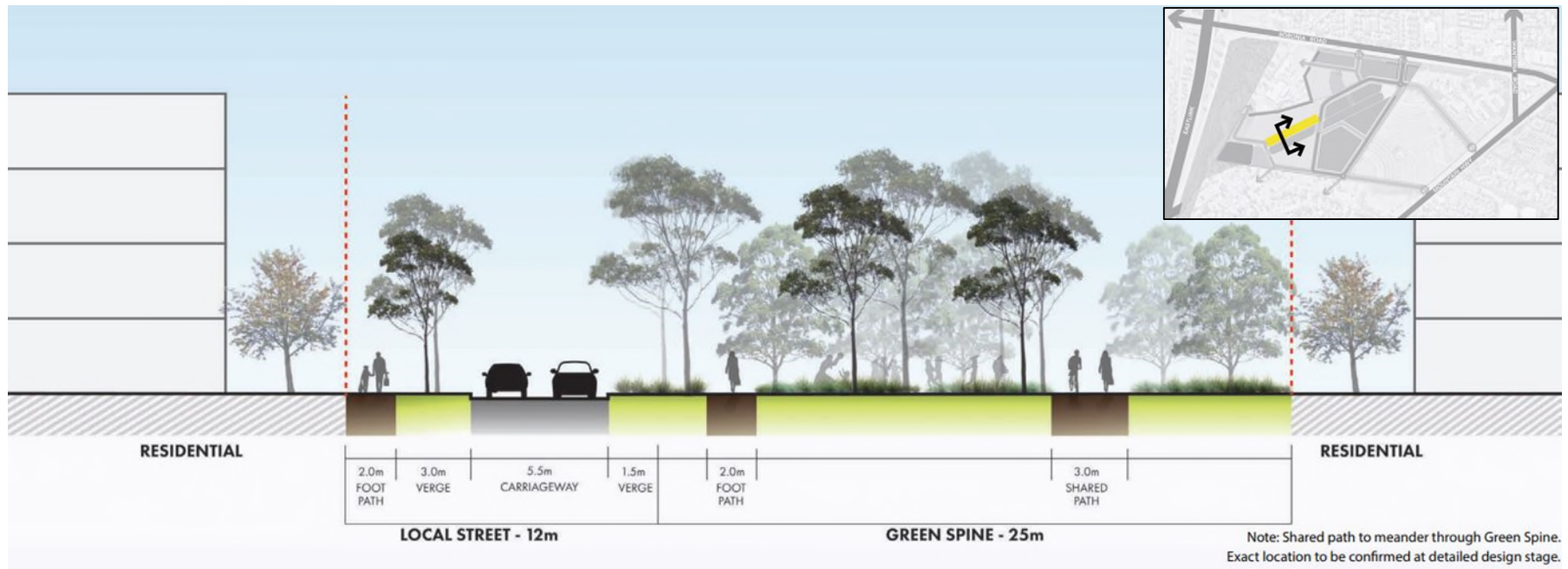


Figure 6 - Green Spine and local street cross-section

