



Department of Transport and Planning

GPO Box 2392
Melbourne, VIC 3001 Australia
www.transport.vic.gov.au

Justin O'Meara
Executive Director – Metropolitan Melbourne
Victorian Planning Authority
Level 25, 35 Collins Street
MELBOURNE VICTORIA 3000

e-mail: justin.omeara@vpa.vic.gov.au

Dear Justin,

DEPARTMENT OF TRANSPORT AND PLANNING RESPONSE TO PUBLIC EXHIBITION- CROSKELL (EMPLOYMENT) PRECINCT STRUCTURE PLAN, INFRASTRUCTURE CONTRIBUTION PLAN AND CASEY PLANNING SCHEME AMMENDMENT C296CASE

I refer to your letter dated 9 September 2024 referring the exhibition of Amendment C296case to the Casey Planning Scheme to implement the Croskell (Employment) Precinct Structure Plan and the Croskell (Employment) Infrastructure Contributions Plan.

The Transport Integration Act 2010 (**TI Act**) establishes a framework for the provision of an integrated and sustainable transport system in Victoria. The Head, Transport for Victoria (**HTFV**) is a statutory body established under section 64A of the TI Act. HTFV's primary objective is to "coordinate, provide, operate and maintain the public transport system and the road system" consistent with the vision statement and transport system objectives.¹

This submission is made under delegation of the HTFV by the Department of Transport and Planning (**The Department**).

The Department has reviewed the draft Precinct Structure Plan (**PSP**) and the associated Infrastructure Contributions Plan (**ICP**), and the associated background documentation prepared by the Victorian Planning Authority (**VPA**). In preparing this submission the Department has identified and considered the existing and ultimate transport network required to serve the PSP area, local area, and wider southeastern region.

This submission is made in relation to transport matters only. It does not include any comments or considerations from the State's Planning Services (formerly DELWP) on the plan and associated documents.

Summary

¹ TI Act, s 64B(1)

The Department is supportive of the overall vision for the PSP subject to resolving the outstanding issues as described below and the comments requiring further consideration listed within Attachment A (Movement Network Comments), Attachment B (Specific Comments).

General Comments

The Department makes the following comments in response to the exhibited documents:

State Transport Network

The Department seeks to ensure that the proposed PSP is consistent with its expectations noting that:

- Thompsons Road, Berwick – Cranbourne Road and Narre – Warren Road, in their ultimate configuration, are planned as 6 lane arterial roads.
- The Department is satisfied, that no additional land is required for these roads through this PSP process. If identified, post PSP, any land requirements, will be managed and addressed through the relevant project.
- The PSP acknowledges the ongoing project requirements for the Thompsons Road / Berwick Cranbourne Road intersection upgrades. Further commentary on changes to the plan to reflect the latest position are provided within this response.
- That the Principle Public Transport Network (PPTN) and Principal Freight Network (PFN) are suitably protected to ensure that their ongoing (and future) operation and function aren't compromised.
- The PSP makes provision for a shared user path (adjacent to Thompson Road) to be delivered within the PSP boundary.
- It makes suitable provision for the delivery of a future bus, cycling, private vehicle, freight and walking networks.
- The provision of a crossing (dealt with as a separate matter) for the Casey Fields Boulevard across the utility easement is supported noting that it will provide opportunity for a local north south connection in provision for local movements, providing resilience to the transport network and removing local trips from the arterial network.

Matters for Further Consideration

Acknowledging the work that has been undertaken to date in conjunction with the VPA in addressing matters that relate to the transport network within and adjoining the PSP area, the Department wishes to pursue the following matters:

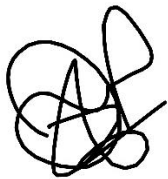
- Whilst supporting the provision of a Shared Path Network Plan within the ordinance (Schedule 16 to Clause 37.07 Urban Growth Zone) further work is required to address multiple comments listed within attachment B of this response. The Department notes that the planning and implementation of this plan is the responsibility of the local authority and not the Head TfV.
- The suitability of raised pedestrian crossings in relation to the interface with the PFN and associated freight movements as well as future public transport movements.
- Understand further the reason for the provision of a left in left out (LILO) access to the open space adjacent to Berwick- Cranbourne Road within the PSP. The Department is seeking to understand how the access will interface with the existing intersection (which current has banned U turns), existing infrastructure within the road reserve as well as its implementation and interface with the road network.

The Department is committed to working with the VPA and stakeholders in the delivery of this PSP and welcome the opportunity to work together to address these matters prior to the referral of unresolved issues to the Standing Advisory Committee for their consideration.

We also recognise that matters relating to the State's transport network may be raised by other submitters in relation to this amendment. The Department reserves the right to comment and address these matters as part of any Panel submission.

If you have any queries regarding this submission, please contact [REDACTED]
[REDACTED]

Yours sincerely

A handwritten signature in black ink, appearing to be 'DZ', written over a circular scribble.

Daniel Zaslona

Manager Transport Network Planning Integration – Land Use
Metropolitan Melbourne
Department of Transport and Planning

21/10/2024

Attachment A – Movement Network Comments

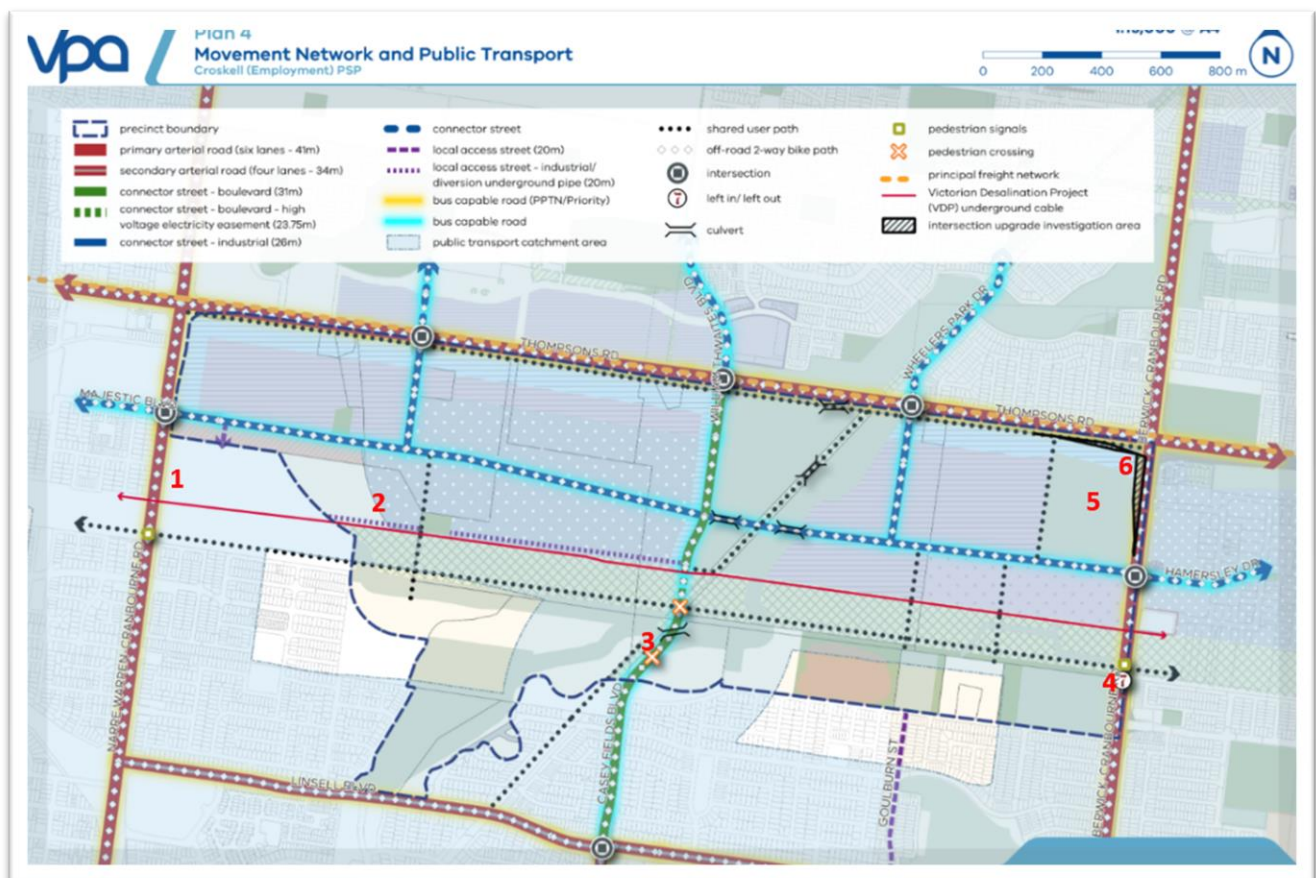


Figure 1. Movement Network and Public Transport Comments

1. The proposed pedestrian crossing as included in the Appendix to the ICP does not connect to the existing footpath located on the western side of Narre-Warren Road. The ICP plans should be revised showing a connection into the existing footpath shown below within Figure 2.
2. There is a local industrial access street connection shown within the residential area. It does not connect into any existing / proposed road networks shown within the plan or within the adjoining area. Please provide further clarification on its location and alignment in relation to the surrounding land uses, including the gap shown within the open space.
3. It appears that there is a gap between the pedestrian crossing and the shared user path that it connects to the south of the easement area.
4. Address matters relating to the feasibility for the provision of a left in left out (LILO) access to the open space adjacent to Berwick- Cranbourne Road.
5. Ensure that any amendments to the location of the retarding basin are referred to the Department for consideration, given its interface with the declared road network and current Big Build Project - Thompsons Road and Berwick-Cranbourne Road Intersection Upgrade.
6. That the PSP boundary be updated to reflect the above big build project, removing the 'investigation area' as this land has since been acquired by the project.

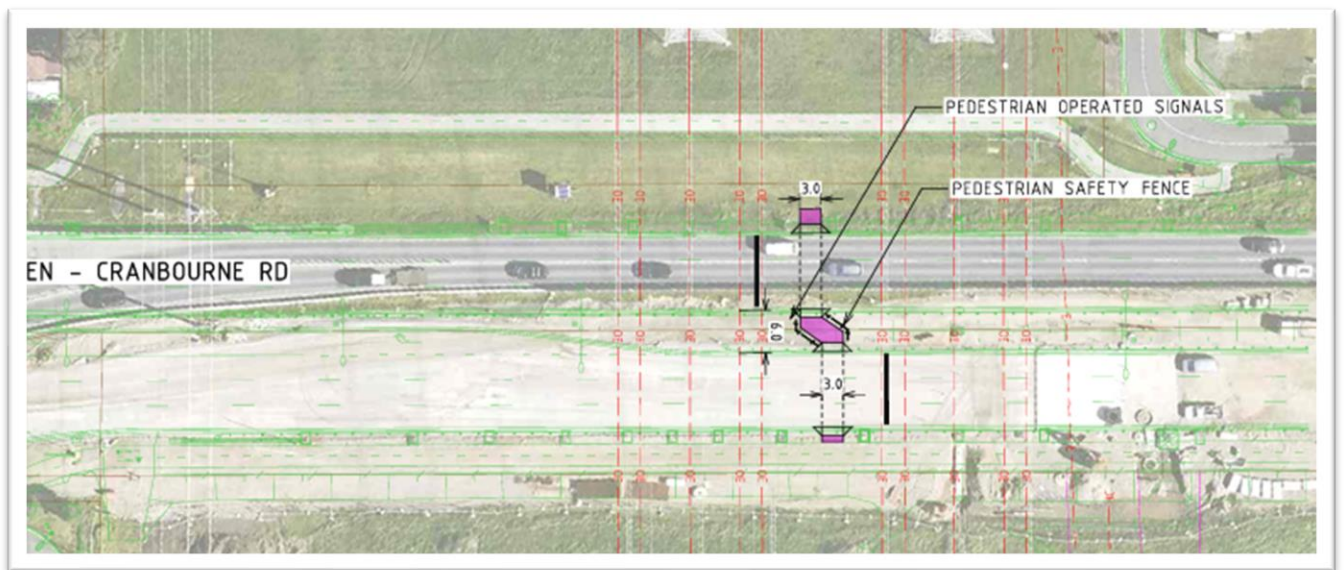


Figure 2. PED-01 Drawing no. -3001

Attachment B – Specific Comments

Croskell PSP September 2024

Page	Comment / Document Text	Proposed Change
Page 9, Paragraph 2	Thompsons road does not yet link to Officer South PSP. This connection is a planned future connection. This should be reworded to reflect that this is a planned connection. The number of lanes are specified for Berwick- Cranborne Road, Narre Warren – Cranborne Road and Linsell Boulevard it should be stated that Thompsons Road is planned to be six lanes.	Proposed rewording: Thompsons Road, a major east-west arterial road and part of the Principal Freight Network that is planned to be duplicated to 6 lanes and extended across the Cardinia Creek to state significant industrial land in Officer South.
Page 13, Place Based Plan	Ensure that any changes for the movement network in Attachment A are reflected within the pages that the place-based plan is shown.	Update Plans to reflect the agreed changes to recommendations under Attachment A.
Page 20 – R4	There are no standard arterial streets to be delivered within this PSP and have no cross sections within Appendix 3 – this wording appears confusing. Recognising that the requirement is standard wording it is suggested that it be amended removing references to arterial roads	Replace first sentence with 'standard road and local streets'
Page 21 – R13	Makes reference to design of arterial roads, there are no arterial roads to be delivered within this PSP	Remove reference to 'arterial roads' from requirement R13
Page 22 – G8	It's unclear what 'alternative shared path approaches' is referring to?	VPA to outline potential alternative shared path options to DTP for consideration as to whether they are suitable.
Page 30 – R18	The sentence appears incomplete. 'Must achieve a minimum of 30%' - of what? What is the 30% that it is covering?	Amend the text to clarify what the 30% relates to
Page 21 – R10	Commercial properties that adjoin declared arterial roads (6 lanes in the ultimate for all adjoining roads) should be accessed via service roads or internal road networks. It is unclear why all forms of access are listed.	Remove all references to other roads other than service road or internal road network.

Page 20 – R9	It's unclear why the pedestrian and active transport infrastructure does not refer to the requirement to prepare a Shared Path Network Plan as per 37.07 Urban Growth Zone – Schedule 16. Suggest that the requirement is updated to ensure consistency on its application and use	Consider revising the requirement to make reference to the Shared Path Network Plan.
--------------	--	--

Infrastructure Contributions Plan

Page	Comment / Document Text	Proposed Change
Page 10, Monetary Component Project Identification	Reference to Error Reference Source Not Found.	Update document to update reference source
Page 12, Transport Construction Projects	Reference to Error Reference Source Not Found.	Update document to update reference source

Infrastructure Contributions Plan Appendix

Page	Comment / Document Text	Proposed Change
General	Croskell is misspelt on all pages.	Correct the spelling with 'Croskell'
General, Raised Pedestrian Crossings	<p>The ICP intersection designs incorporate raised pedestrian crossings. Further discussions with the VPA are required regarding the suitability of raised crossings in relation to the interface with the PFN and future public transport movements.</p> <p>The Department refers to Road Design Note RDN 03-07 – Raised Safety Platforms in relation to heavy vehicles at 6.6.</p>	Discuss potential amendments with the Department

Planning Scheme Ordinance

Page	Comment / Document Text	Proposed Change
------	-------------------------	-----------------

Page 1	Update Plan 1 to show latest Place Based Plan within the PSP	Substitute plan to reflect the current plan and changes requested
37.07 Urban Growth Zone, Schedule 16, Page 5, Shared Path Network Plan	<p>Noting that the Head, TfV is not identified as either a referral or determining authority, The Department makes the following observations and comments on the proposed Shared Path Network Plan:</p> <ul style="list-style-type: none"> • The intention of the inclusion of this provision is unclear. Further clarification around the intent and benefit of the plan should be outlined within the ordinance. • It's unclear whether this plan is applicable to all applications for subdivision within the PSP area. Currently the proposal appears to be tied to Thompsons Road only. • The term shared user path refers to a specific type of active transport infrastructure and is considered not suitable if the plan will incorporate other types of pathways including footpaths and cycling paths. Therefore, it is recommended that the name be changed from <i>Shared Path Network Plan</i>. <p>The text does not clearly identify which roads the requirement intended to apply to. It is noted that:</p> <ul style="list-style-type: none"> • It is unclear if the plan would physically apply to the declared arterial road network that adjoins the PSP area. • Why Thompsons Road has specifically been identified within the text and whether it specifically has a relationship with the proposed plan and why Narre Warren Road and Berwick Cranbourne Road have not specifically been identified. <p>The reference to bicycle paths should be changed to cycling paths.</p> <p>With regards to the third dot point the following should be considered:</p>	<p>Recommend that the Department seeks for the drafting to be updated based on the comments provided. Terminology should be consistent and clearly identify types of infrastructure required to be included/considered and provide clear direction about which roads trigger the requirement of this plan.</p>

	<ul style="list-style-type: none"> It isn't clear who is responsible for the delivery of the paths or how developers can deliver paths on land that is outside of their ownership. It is not clear how developers can design footpaths and appropriate locations of footpaths if interim or ultimate designs for the road have not been resolved. 	
Page 10 – Conditions – Subdivision or buildings and works permits where land is required for community facilities, public open space, or road widening	Remove reference to '(excluding widening under a Public Acquisition Overlay for road widening)' as the land within the PAO adjoining the site has been acquired through an earlier big build project – Thompsons Road upgrade Stage 1	Remove text (<i>excluding widening under a Public Acquisition Overlay for road widening</i>)
37.07 Urban Growth Zone, Schedule 16, Page 11, Shared Path Network Plan	<p>The Department makes the following observations and comments on the proposed Shared Path Network Plan:</p> <ul style="list-style-type: none"> Terminology should be updated based on any changes made to page 5 as described above. What is the reference to a 'masterplan' inferring? It is not clear who is responsible for delivering the path network. Is it Council or the developers / landowners? 	As above terminology should be clear and consistent. It is unclear what the masterplan reference is referring to. The implementation of this condition needs to be resolved.
37.07 Urban Growth Zone Schedule 16 Page 14 Shared Path Network Plan	Amend terminology as previously suggested on page 5	Amend terminology as previously suggested on page 5

Integrated Transport Assessment

Page	Comment / Document Text	Proposed Change
Page vi, dot point 1 Target 6	Typo missing word: proposed... are in alignment.	Typo, please correct it is unclear if the missing word is cycling paths or shared user path.
Page 10, figure 5	There is a funded extension of route 798 through to Clyde North, via Hardys Road.	Update map and text to reflect extension of this route

Page 16, final dot point.	The 863 and 895 should be updated as in operation.	Update the text to reflect that these routes are now in operation.
Page 19, figure 9	The cross section for the local access street – industrial is listed as 20m in the legend but appears as 22m in Figure 12.	Clarify and provide the correct legend based on the width provided in the cross section.
Page 45, dot point 4 under additional considerations	<p>The bus routes servicing this PSP are not considered to be long distance bus routes.</p> <p>Change language as follows:</p> <p>Improve access by public transport by extending existing bus routes the precinct and establishing higher frequency services on long distance direct bus routes</p>	Update as suggested
Page 45 additional considerations	<p>Recommended addition to additional considerations.</p> <ul style="list-style-type: none"> Ensure adequate continuous lighting to support all day/night travel periods (from both wayfinding and personal security point of view). 	Incorporate recommended addition.
Page 31,39 and 45, reference to raised pedestrian crossings	Raised crossings can present safety issues for freight and buses. As the PSP will include industrial land uses and adjoins the PFN it is unclear how frequent freight movements been considered with the inclusion of the raised pedestrian crossings.	<p>The Department requires detail on how the incorporation of raised safety platforms within the intersections in the ICP appendix have considered the proposed industrial land use in relation to freight and consideration of bus movements.</p> <p>The Department refers to Road Design Note RDN 03-07 – Raised Safety Platforms in relation to heavy vehicles at 6.6</p>

Background Documents

Page	Comment / Document Text	Proposed Change
Drainage Strategy Concept Design Report June 2024 Page 65	The Department understands that the retarding basin adjoining Berwick-Cranbourne Road may be relocated. It is noted that the currently location of the retarding basin identified in Figure 10 may be relocated.	Any alterations to the location of the basin must be discussed and agreed with the Department to ensure the existing and future drainage management schemes from arterial roads isn't affected.

