

PSP2.0

Officer South (Employment)

BUNURONG COUNTRY

Precinct Structure Plan

NOVEMBER 2024

ACKNOWLEDGMENT OF THE TRADITIONAL OWNERS

The Victorian Planning Authority proudly acknowledges Victoria's Aboriginal community and their rich culture and pays respect to their Elders past and present.

We acknowledge Aboriginal people as Australia's first peoples and as the Traditional Owners and custodians of the land and water on which we rely.

We recognise and value the ongoing contribution of Aboriginal people and communities to Victorian life and how this enriches us.

We embrace the spirit of reconciliation, working towards the equality of outcomes and ensuring an equal voice.

The Officer South (Employment) Precinct is located on the traditional lands of the Bunurong People. The Bunurong People are represented by the Bunurong Land Council Aboriginal Corporation (BLCAC).

We acknowledge the Bunurong People as the Aboriginal Traditional Owners of their unceded Country. We acknowledge their ongoing connection to this land, and we pay our respects to their Elders past and present.

'Cardinia' has been identified as the anglicised interpretation of an eastern Kulin term roughly translated to "looking east from the creek" (BCLAC)

We thank BLCAC for their engagement throughout this project.

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1.0 CONTEXT

1.1 How to read this document

The precinct structure plan (PSP) guides land use and development where a planning permit is required under the Urban Growth Zone (Clause 37.07 of the Cardinia Shire Planning Scheme), or any other provision of the Cardinia Shire Planning Scheme that references this PSP.

The PSP is structured as follows:

Part 1 – Context

The first part contains an overview of the PSP document and the function of individual components as well as infrastructure contributions required to deliver the precinct

Part 2 – PSP outcomes

A planning application and subsequent planning permit must implement the outcomes of the PSP.

The outcomes are expressed as:

- The **vision**, which is the overarching unique place-based outcome intended for this PSP
- The **purpose**, which sets out how the PSP will deliver on the vision for the precinct
- The **place-based plan**, which is a plan of the future community

The **PSP performance summary** illustrates performance against the targets set out in the PSP Guidelines. Where targets are not met, the PSP identifies the reasons why the targets have not been met, and what alternative approaches have been adopted to comply with the PSP Guidelines principles.

Part 3 – Implementation

Each **hallmark** is implemented according to the following sub-structure:

- **PSP objectives:** The PSP identifies a set of place-based objectives to achieve the vision and purpose for the precinct. These provide the guidance required to achieve the specific outcomes sought for each of the seven hallmarks within the precinct
- **PSP implementation and delivery:** To assist with understanding how to implement the strategies, the PSP provides guidance in the form of requirements, plans, tables and diagrams
- **Place-based requirements and guidelines:** Provides decision-making guidance when exercising discretion to determine if an application meets the relevant requirement.

Alternative strategies to achieve the vision and objectives may be considered by the responsible authority. Alternative strategies must demonstrate how they will achieve the vision and objectives of the PSP.

To assist with understanding where alternative approaches may be supported, **plans, tables and diagrams** identify the level of flexibility that may be considered.

Not every aspect of land use, development or subdivision is addressed in this PSP. A responsible authority may manage development and issue permits as relevant under its general discretion. The [Generally in Accordance Guidance Note](#) is available on the VPA website to provide direction in the application of discretion where a PSP applies.

Development must comply with Acts and approvals where relevant e.g., the *Environment and Biodiversity Conservation Act 1999* or the *Aboriginal Heritage Act 2006*, among others.

PSP Guidelines refers to the **Precinct Structure Planning Guidelines: New Communities in Victoria**, available on [the VPA website](#)



1.2 Objectives, requirements and guidelines

A planning application and subsequent planning permit must implement the outcomes of the PSP. The outcomes are expressed as the PSP Vision, PSP Purpose and Objectives in the following chapters.

Each chapter of the PSP contains requirements and guidelines as relevant. Requirements must be adhered to in developing the land. Where they are not demonstrated in a permit application, requirements will usually be included as a condition on a planning permit however they may not be of the same wording as in the structure plan. A requirement may reference a plan, table or figure in the structure plan.

Guidelines express how discretion will be exercised by the responsible authority in certain matters that require a planning permit. If the responsible authority is satisfied that an application for an alternative to a guideline implements the outcomes and /or objectives, the responsible authority may consider the alternative. Alternative solutions put forward must be generally in accordance with the PSP. A guideline may include or reference a plan, table or figure in the PSP.

Meeting these Requirements and Guidelines will implement the Vision, Purpose and Objectives of the PSP.

1.3 Purpose of the PSP

The purpose of the PSP is to activate the PSP outcomes by:

- Providing the planning conditions for private industry delivery
- Identifying and, where appropriate, providing the shared funding for a diverse range of open spaces and community infrastructure
- Ensuring planning permit applications will generate the population to support investment in critical infrastructure.

1.4 Regional and strategic policy context

Plan Melbourne 2017–2050, the *Melbourne Industrial and Commercial Land Use Plan* (MICLUP) and the *South-East Growth Corridor Plan* (2012) have identified the Officer South (Employment) Precinct to support a State Significant Industrial Precinct and Regionally Significant Commercial Area (including business with residential uses). These policies envisage the primary role for the precinct to cater for an employment and economic role, providing a long-term supply of employment land as part of a broader Officer–Pakenham employment corridor in Melbourne's south-east (refer [Plan 1 Regional Context](#)).

1.5 Precinct features

The Officer South (Employment) Precinct is bound by Cardinia Creek to the west, Lower Gum Scrub Creek to the east, Princes Freeway to the north and green wedge to the south. The precinct area abuts several emerging growth area communities including Cardinia Road (Employment) PSP to the east, Minta Farm, Clyde North, Cardinia Creek South PSP to the west, and Officer PSP to the north.

The natural watercourses form a large transition zone between this precinct and the neighbouring communities to its east and west, providing a high-quality cultural and landscape amenity opportunity for its future population to enjoy. Aboriginal heritage areas of sensitivity are identified throughout the precinct and a cultural heritage area is identified along Cardinia Creek, to be enhanced and protected through the PSP.

The VPA has consulted with Bunurong Land Council Aboriginal Corporation as the Traditional Owners of the land to help ascertain the important cultural values that are significant in the precinct. The Bunurong people's heritage and story play an important role within the PSP, guiding outcomes that recognise the celebration of water, their journey (from the Dandenong Ranges to the south), Cardinia Creek life and the congregating of people and communities. Open spaces have been planned at identified high ground meeting places and connecting active path networks celebrate their journey in the east–west and north–south directions.

Two significant easements, a high-pressure gas and an electricity transmission easement, further divide the precinct into three approximately horizontal parts, which carry significant development constraints. There are also existing NBN fibre optic cables, additional gas distribution mains and high to very high retention value trees throughout the precinct.

A major existing sewer pump station is located in the north of the precinct to provide wastewater services to the precinct and Officer. South East Water has proposed a further sewer pump station in the south of the precinct along Patterson Road to service the PSP catchment.

Public Acquisition Overlays (PAO) have been applied for the city bound on-ramp from Officer South Road to the Princes Freeway and for potential future widening along Thompsons Road. The Floodway Overlay (FO) north of Lecky Road at Lower Gum Scrub Creek has been amended to accommodate future infrastructure and updated floodway data, as detailed by the planning ordinances.

[Plan 2 Precinct Features](#) illustrates the existing natural and cultural features. These precinct features guide and influence the character of the precinct structure and its future neighbourhood, namely through:

- Aboriginal heritage areas of cultural sensitivity and landform
- Existing high value vegetation
- Environment Protection and Biodiversity Conservation (EPBC) protected species
- Post-contact heritage features and values
- Water features
- Treed view-lines and views to the Dandenong Ranges.

Water will be a positive, visible, and significant contribution to the public realm and open space, including at key gateways of Thompsons Road, Grices Road and Lecky Road bridges and along the Officer South Road north-south gateway corridor. Celebrating water in the landscape will have multiple benefits by slowing and storing stormwater, including significant stormwater management strategies to protect the Koo Wee Rup Levy, and providing ecological regeneration and enhanced biodiversity. Significant changes of level within the Cardinia Creek corridor present areas where development could be restricted due to flooding and drainage constraints.

Areas downstream of the precinct presently experience flooding impacts in minor rainfall events. This includes flows from the precinct itself, but also flows from adjoining areas to the north and east of the precinct which will be treated, retarded and/or diverted through infrastructure to be delivered. Management of the catchment to the east of Officer South Road is dependent upon the construction of the Gum Scrub Creek Priority Works (shown on [Plan 9 Integrated Water Management](#)) outfall to divert volumes from Gum Scrub Creek. Development within the eastern portion of the precinct will be limited until the delivery of the Gum Scrub Creek Priority Works.

The precinct has also been home to rural lifestyle and agricultural uses, including farming, horticulture, and equine industries. The potential extension of existing equestrian trails from the Cardinia Creek Parklands and the City of Casey through the precinct may provide further support to the regional equestrian community, promoting tourism for the local economy.

The precinct contains significant ecological features including habitat to be protected for matters of national environmental significance. The creek corridors bounding the precinct to the east and west provide important biodiversity corridors. Eastern Grey Kangaroos are present in the local rural and natural areas and development of the precinct should consider the management of kangaroos.

There are two identified post-contact heritage sites located within the precinct, being Cardinia Park, Former Gin Gin Bean Pre-emptive Right Site at 410 Officer South Road, Officer ([HO91](#)), and four trees and one outbuilding at Jesmond Dene at 425 Officer South Road, Officer ([HO92](#)).

In addition, the site at 90 Handford Lane, Officer is on the Victorian Heritage Inventory ([H7921-0129](#)) as it contains historical archaeological remains including artefact scatters, exotic plantings and building remains.

The Officer South (Employment) PSP has been prepared to ensure the seamless integration of the precinct into its key cultural, natural, and urban features.

Refer to the [Officer South \(Employment\) Precinct Background Report](#) for a more detailed discussion of regional context and features that have informed spatial planning outcomes sought by this PSP.

1.6 Infrastructure contributions plan

The legal framework for the infrastructure contributions plan (ICP) system is primarily set out in the *Planning and Environment Act 1987* (P&E Act). The main component of the ICP legislation is contained in Part 3AB of the P&E Act.

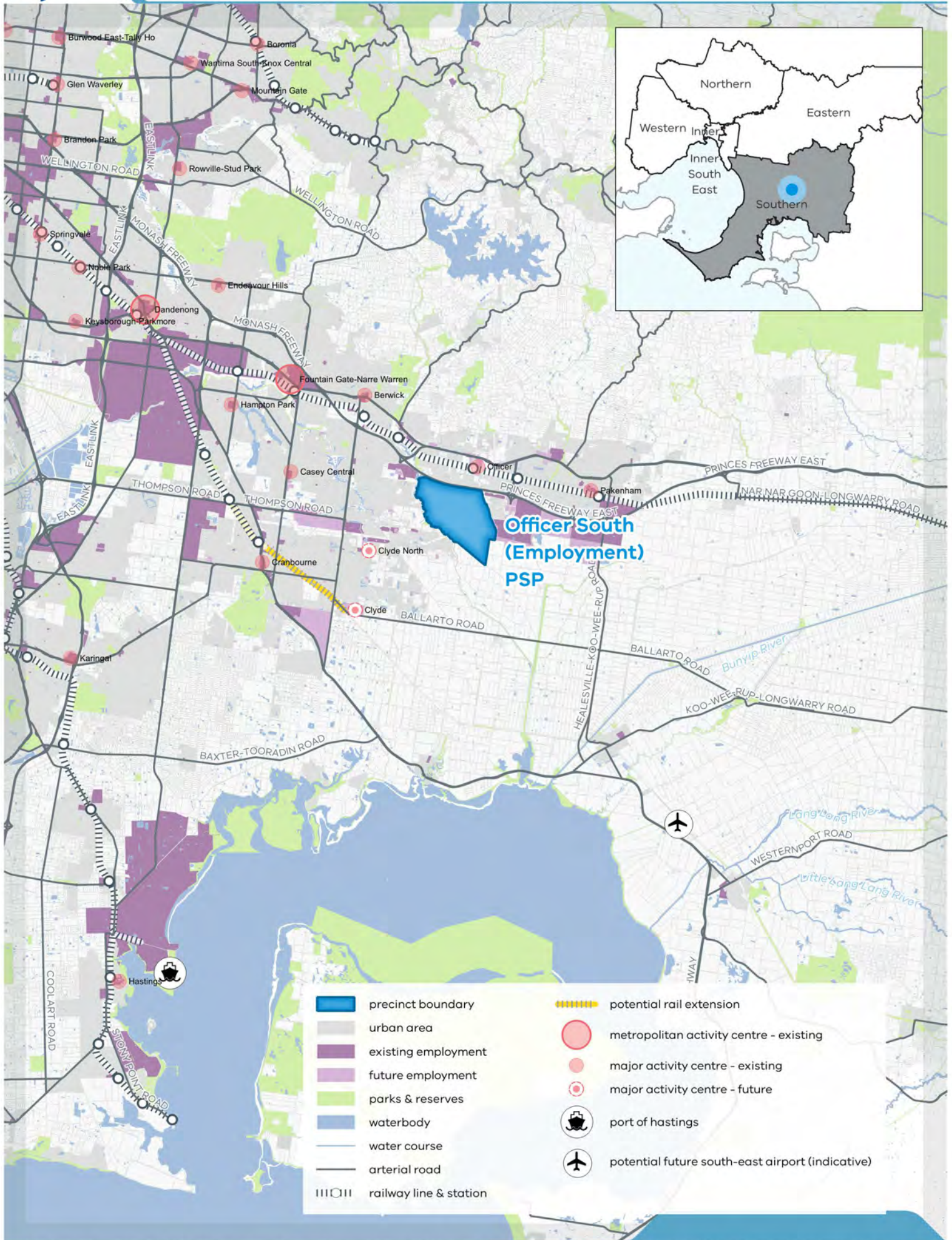
Development proponents within the Officer South (Employment) precinct will be bound by the Officer South (Employment) Supplementary Infrastructure Contributions Plan (the ICP). The ICP sets out requirements for infrastructure funding across the Officer South (Employment) precinct and will be incorporated in the Cardinia Planning Scheme.

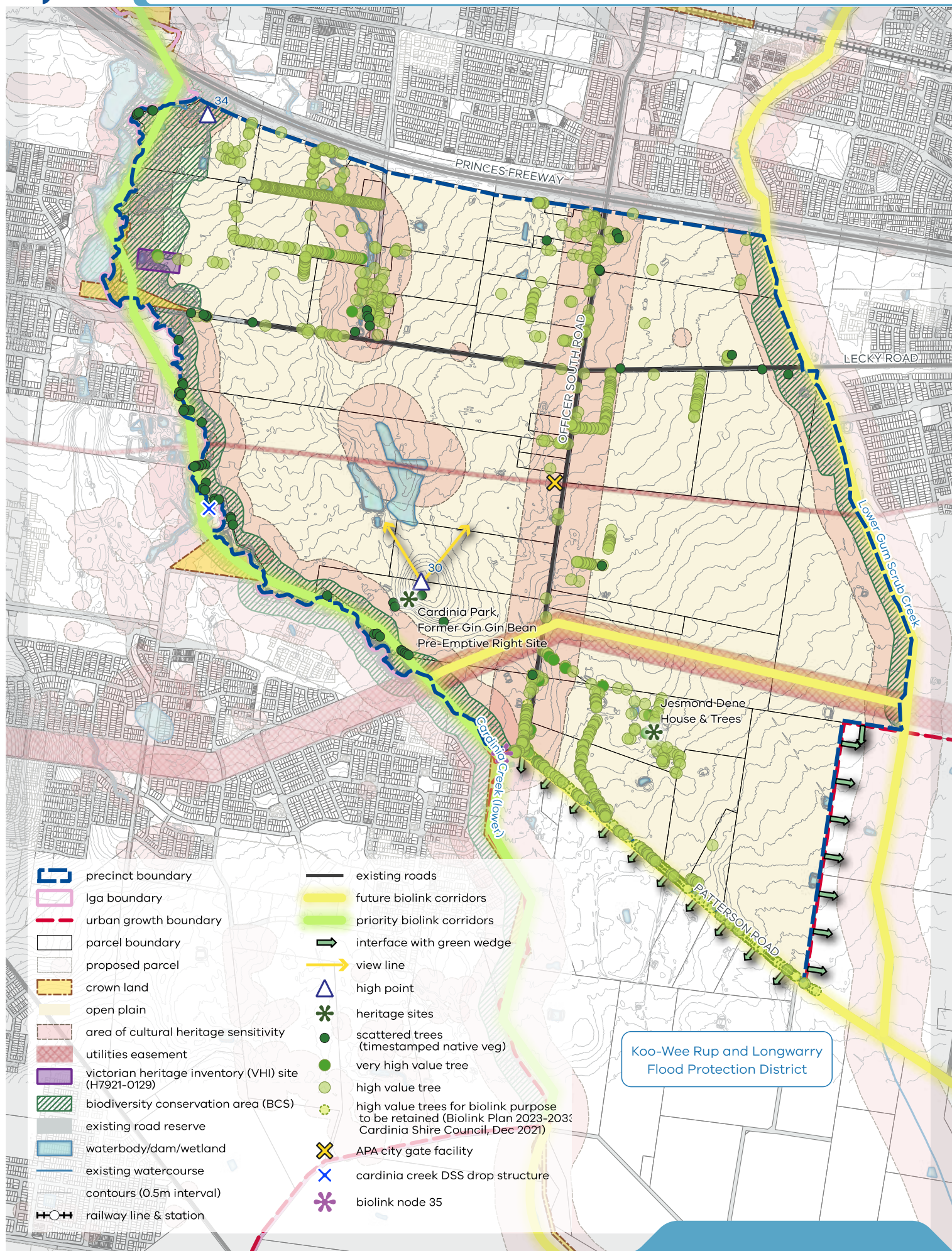
1.7 Background information

The [Officer South \(Employment\) Precinct Background Report](#) provides detailed background information relating to the precinct, including its strategic and local context, physical attributes and provides a detailed account of the VPA's position on various challenges in the precinct based on the 20-minute neighbourhood principles.

The background report highlights considerations and research that have informed the preparation of the PSP, including the key findings from the various technical studies such as arboriculture, drainage, open space, transport, utilities, economics, and community facilities.

The *Officer South (Employment) Precinct Background Report* with the associated technical report summaries is available on the [Officer South \(Employment\) webpage](#). The technical studies are available at <https://vpa.vic.gov.au/project/officer-south-employment/#supporting-documentation>.





2.0 PSP OUTCOMES

2.1 PSP vision

Officer South (Employment) will become an innovative and sustainable employment precinct that will deliver a high amenity regionally significant commercial area and state significant industrial precinct, combined with a modern and diverse urban living hub to form a unique working and living environment.

The precinct forms the core of the Officer–Pakenham State Significant Industrial Precinct and will deliver south-east Melbourne’s next generation of industrial, manufacturing and logistics enterprises estimated to bring approximately 22,000 diverse jobs closer to emerging communities in Cardinia Shire and the City of Casey.

The urban structure promotes large and intensive industrial land uses within the core of the precinct, making Officer South (Employment) the prime location for heavy industries to thrive in the south-east region. It will attract primary industries such as agriculture and mining, as well as next-generation manufacturing for knowledge-based and health care sectors. Complemented by a sleeve of light industrial, and freight and logistics industries, commercial businesses will be able to leverage off industries to take full advantage of the new and exciting employment landscape to inspire innovation.

Excellent connectivity on a north–south and east–west axis connects Officer South (Employment) to major infrastructure. The north–south axis connects to the Officer Major Activity Centre, Officer Train Station and a potential future south-east airport. The precinct provides significant freight movement via Thompsons Road allowing direct access to broader south-east employment precincts and population centres.

Employment-focused land integrates with a compact 20-minute neighbourhood forming part of an expanded Cardinia Road community. The residential precinct provides a range of housing options and community facilities, creating a vibrant and diverse neighbourhood for workers and residents. It is estimated that Officer South (Employment) will be home to around 5,000 residents with a walkable, high amenity environment, drawing on a character green spine that links to key destinations anchored by active open space. The local town centre and community hub provide extensive local opportunities for shopping, education, community services, and recreation.

Officer South (Employment) will protect conservation areas along the high valued waterways to ensure the protection of habitat for matters of national environmental significance. The natural features of Cardinia and Lower Gum Scrub Creeks will benefit the workers and residents within the precinct and are protected by a complex water management system. The unique natural attributes of the precinct, quality path networks and local parks create an environment conducive to discovery of highlighted heritage and cultural values areas within Officer South (Employment).

A beacon for climate change resilience through sustainability and innovation, integrating a high amenity built environment and a significant active transport network, Officer South (Employment) is the new template for employment precincts of the future. This precinct exemplifies the principles of living locally and will be a primary choice for establishment of industry and businesses of the future.

While not being prescriptive, [Figure 1 Sub-precinct concept plan](#) is intended to encourage the achievement of the PSP vision on a sub-precinct basis.

Figure 1

Officer South (Employment) Sub-precinct concept plan

Princes Enterprise Precinct

Located along the Princes Freeway it enables high exposure to passing traffic and promotes a strong connection to Officer and the adjoining commercial precincts. Its Industrial 1 and Commercial 2 zoning provides the ability to accommodate a diverse mix of industries, including potential **research and innovation premises, warehousing and premium logistics and transport**-based industries.

Cardinia Creek Precinct

The land abutting Cardinia Creek is strategically important providing quality natural amenity to surrounding areas. Complemented by the adjacent wetland and interfacing with the cultural value area, this precinct will provide a high-quality State-Significant Industrial Precinct (SSIP) industrial interface to surrounding high amenity areas, and industrial buffering to the western residential communities. It provides flexibility to the SSIP, allowing for smaller to medium lots to be developed to respond to market demand, helping to accommodate high profile light industry operations with a customer focus and high quality presentation to industries such as **wood products and furniture manufacturers, textiles, IT industries and music industries**.

Commercial Gateway Precinct

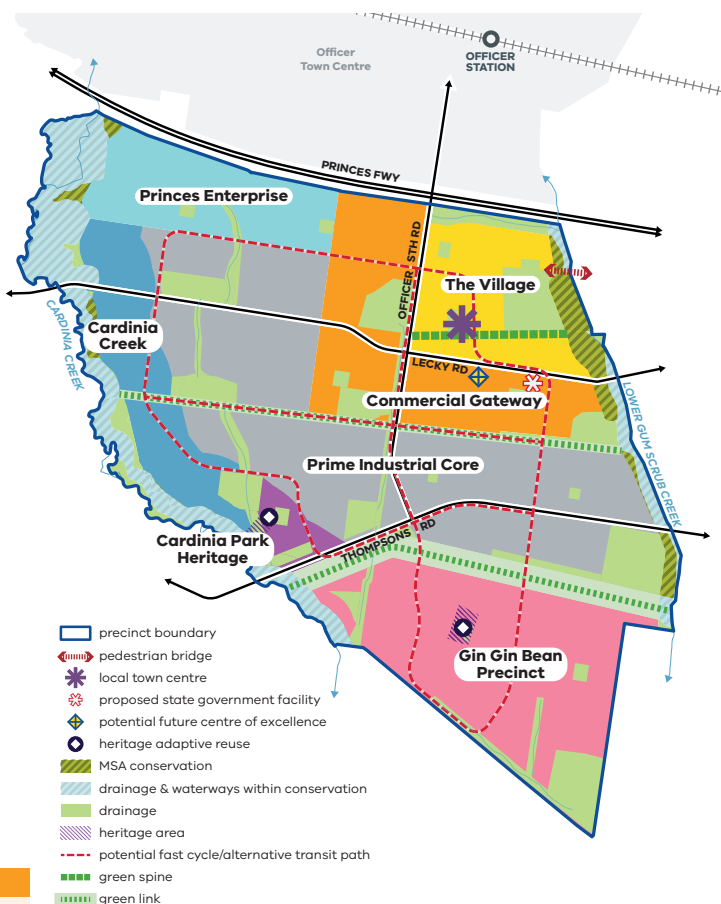
Premium quality service and commercial frontage to Lecky Road and Officer South Road, this precinct provides a quality northern gateway, complements the residential interfaces and provides a clear transition between intensive industries and proposed Officer South township. The precinct will provide small to medium lots with high quality frontages to Lower Gum Scrub Creek. The precinct will accommodate **service industries and business parks** that will benefit from arterial connections with wider communities and businesses. The inclusion of a proposed state government facility near the local town centre will drive an innovative precinct suitable for **research and development** centres. Facilities such as the proposed industry training institute (Centre of Excellence) are encouraged in commercial/mixed use areas where the sites have high amenity and are well-served with access to transport and community facilities.

Cardinia Park Heritage Precinct

Existing heritage buildings including very high-quality trees are a key feature to the Officer South (Employment) precinct. Opportunity lies for these local heritage protected sites to be re-purposed into business capital ventures and to set this precinct apart from other industrial centres in the south-east corridor. Closely located with neighbouring heavy industry and complemented by the adjacent wetland and interfacing with the cultural value investigation area, this precinct will provide much-needed relief to the working environment, repurposed as **industry information centres, integrated upmarket public supply depots, plant nursery and even potential breweries**. The adaptive re-use of these premises will provide an opportunity for standout industrial business with no adverse impacts to the surrounding industrial uses.

Green spines/links

An abundance of green links solidify Officer South (Employment) as a new order employment precinct that offers bio-links, wetlands and retention of age-old trees to help mitigate the impacts of an industrial environment to climate change. Officer South Road is to be recognised as a key significant transport link that will promote adjacent green and blue networks. The gas and electricity easements will accommodate numerous plant species encouraging flora to thrive and fauna to move between the two major creeks that hug Officer South (Employment) in the north-south axis. The old Lecky Road alignment has been repurposed to a green spine that links several key destinations such as Lower Gum Scrub Creek and extensive wetlands, the local primary school and active open space, the neighbourhood centre and village square.



The Village

The precinct will be an **integrated and connected neighbourhood** that provides for a range of diverse housing options, including small lot housing, rear-accessed townhouses and apartment living, to promote **key worker housing** to the employment area and supporting living locally. An active **green spine** promotes a healthy lifestyle linking the adjoining community at Cardinia Road Employment precinct to the abundance of amenity via walking and cycling networks and bringing communities together with spaces for social events. Delivering two schools, a local town centre and premium open space, this precinct will bring 24-hour life to the State and Regionally Significant employment hub.

Prime Industrial Core Precinct

A robust area of SSIP safeguarded by an Industrial 1 zone and surrounding commercial and light industrial development, this precinct will provide large scale parcels for heavy industrial enterprise to excel. A key part of Officer South (Employment), its strong connections to the principal freight network will attract industries such as **advanced manufacturing and engineering** for the construction and transportation industries, **scientific research and chemical production, aerospace**, and others that may require threshold distances and large land parcels.

Gin Gin Bean Precinct

Located next to the green wedge farming land and including the Gin Gin Bean Pre-Emptive Right Site this precinct has a premium quality interface to the southern farmland and urban growth boundary. The sub-precinct will offer key opportunities to **food and fibre** industries to leverage farming and agricultural businesses of the green wedge, and provide opportunities for the distribution of goods and services to local communities. The transmission easement can provide urban farming opportunities and ancillary uses such as public car parking, while water intensive industries are able to leverage off substantial drainage assets in this precinct.

Alternative mode of transport network

Opportunity for a high-speed, signposted e-scooter/cycle network and alternative transport, or autonomous shuttle route with electricity charging infrastructure. An east-west link via Lecky and Grices Roads as well as a north-south link to Officer will provide high grade access to key destinations of Officer Train Station and Minta Farm Innovation precinct.

2.2 PSP purpose

1: Provide for employment and industries of the future

Officer South (Employment) provides a Regionally Significant Commercial Area (RSCA) and a State Significant Industrial Precinct (SSIP) as part of the broader Pakenham–Officer SSIP corridor, south-east Melbourne's future premier economic and employment hub.

The employment landscape has significantly changed in recent years, shifting the standard employment model to a work–life focused environment. Officer South (Employment) is creating an employment precinct that focuses on the new movement and place of our working environment, delivering a high amenity residential area which encourages residents to walk, cycle and scooter to work via an efficient active transport network.

An ample supply of industrial land across the precinct will allow for businesses to drive the diversity of industries and their location. The configuration of the commercial land ensures that the SSIP is safeguarded from the new community area, and provides a wide array of opportunities including retail, services and offices that will activate the frontages of Officer South Road, Lecky Road, and the Princes Freeway.

Employment areas are configured to capitalise on the locational attributes that will support industries of the future. Large land parcels and direct access to the high voltage power network and where needed, the gas network and renewable gas, encourages and supports industries to expand their businesses, and additionally leverage off access to the key freight corridor of Thompsons Road and beyond to the potential future south-east airport.

2: Value-add to existing communities

The precinct delivers a diverse range of knowledge-based, manufacturing, logistics and agribusiness enterprises for existing and emerging communities, meaning surrounding residents have more job choice and can work closer to home.

The precinct's high amenity and well-designed residential neighbourhood provides the population to deliver broader community needs, including two primary schools (government and non-government), local town centre, and sports reserve. The new housing diversity PSP targets ensure that there are more housing choices which will unlock options for key worker and affordable housing within a walkable catchment.

Additional mixed-use precincts around local parks plan for a connected community that can take advantage of small local eating and work opportunities at key destinations. These small sub-hubs allow for potential small workshop/shared workspaces, eateries and shop-top residences to create an active, practical, and vibrant environment.

3: Release key transport/economic links

Officer South (Employment) incorporates strategic planning for the alignment of Thompsons Road, a pivotal east–west arterial freight route that will unlock the Pakenham–Officer SSIP. In addition, Grices and Lecky Roads will provide essential east–west connections to the adjoining residential precincts in Cardinia Road (Employment), Minta Farm and Clyde North enabling residents good access to employment and surrounding community destinations. Within the precinct, the active and public transport links connect surrounding townships of Officer, Berwick, and Pakenham.

4: Integrate complementary community and open space assets

Officer South (Employment) will capitalise on the existing and proposed amenity and recreational values of green spaces within utility easements, Cardinia Creek, Lower Gum Scrub Creek, and future drainage assets. Its active and passive open space networks provide key destination points within the precinct, to enable community and employees of the broader region to access and use these areas for leisure and recreation purposes via its connected road, active and public transport networks. The precinct provides for the establishment of Biodiversity Conservation Strategy (BCS) conservation areas to ensure the protection of connected habitat for Growling Grass Frogs within the precinct.

5: Respond to sustainable environmental and drainage needs

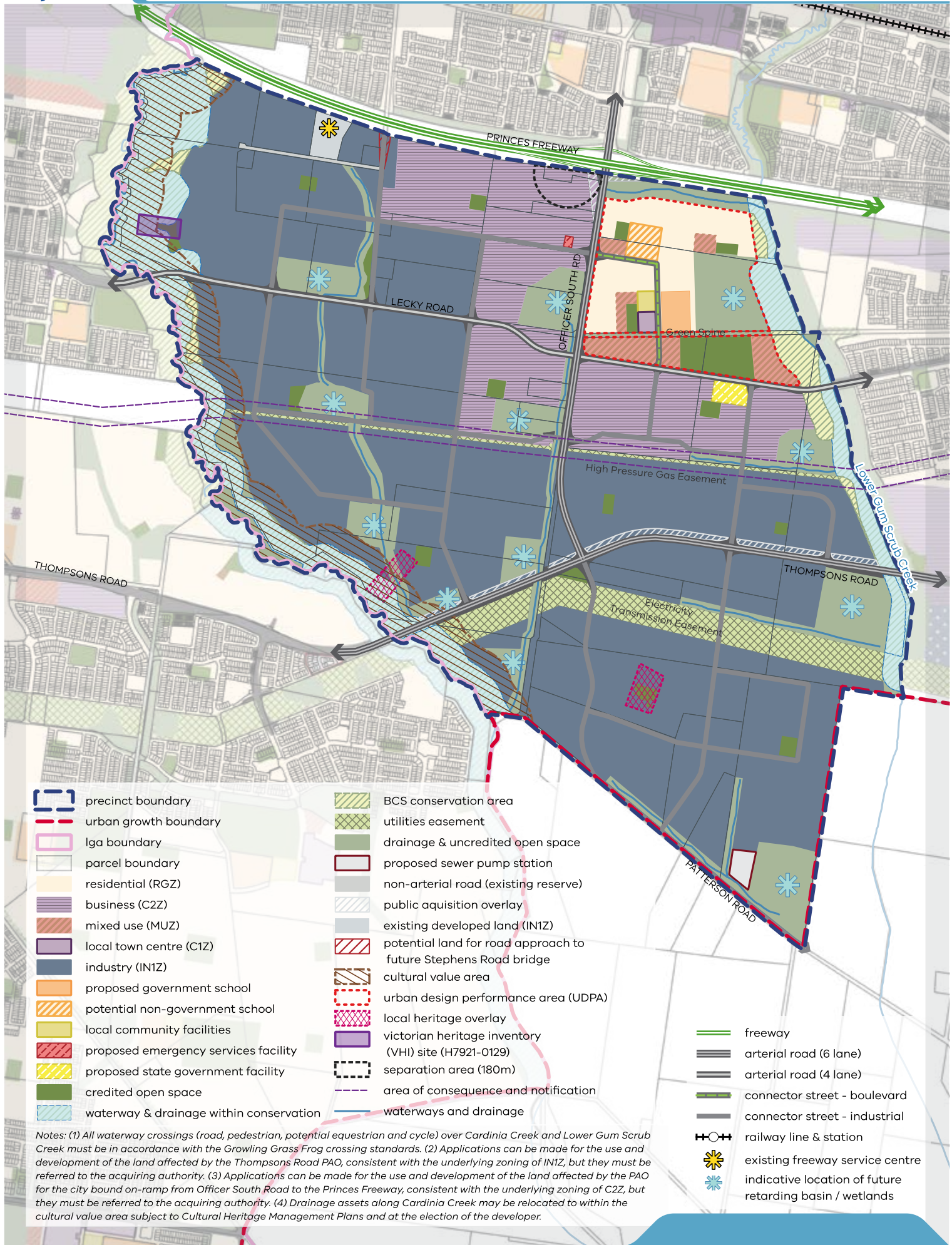
Officer South (Employment) fosters the coordinated delivery of servicing and on-site integrated water management opportunities to facilitate a sustainably responsive precinct. The precinct's integrated water management (IWM) strategy outlines the priorities available to ensure stormwater management and water use can be considered in the design and delivery of built form, streets, open space, and drainage assets. These opportunities are subject to further innovation, supported by future investigations into 'closed loop' water systems and stormwater harvesting and reuse.

The precinct will contribute to the long-term conservation of significant flora and fauna species through establishment and protection of habitat, particularly within BCS conservation area 36, providing high quality connected habitat corridors.

6: Future-proof industry-ready services and infrastructure

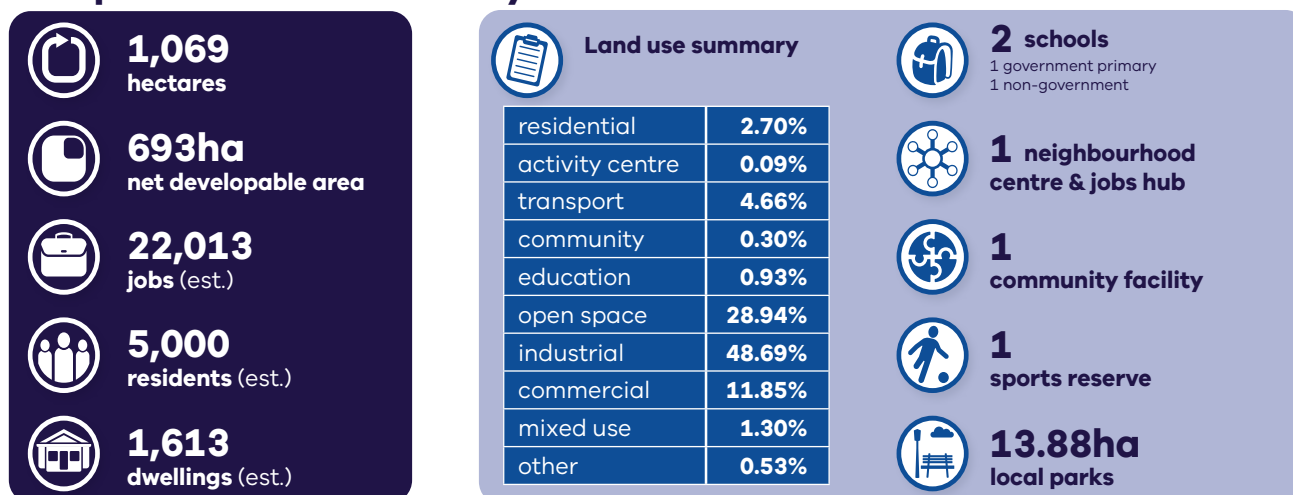
The Officer South (Employment) PSP implements appropriate planning mechanisms to facilitate innovative utility service systems and infrastructure design and delivery. These delivery mechanisms encourage consideration of opportunities for green star construction, carbon reduction, clean energy generation and smart city technology through development of its utility networks, built form, parks and transport solutions.

The PSP provides for the staging of development of the precinct that aligns with infrastructure provision which will also assist with the forecasting of development to plan for future services.

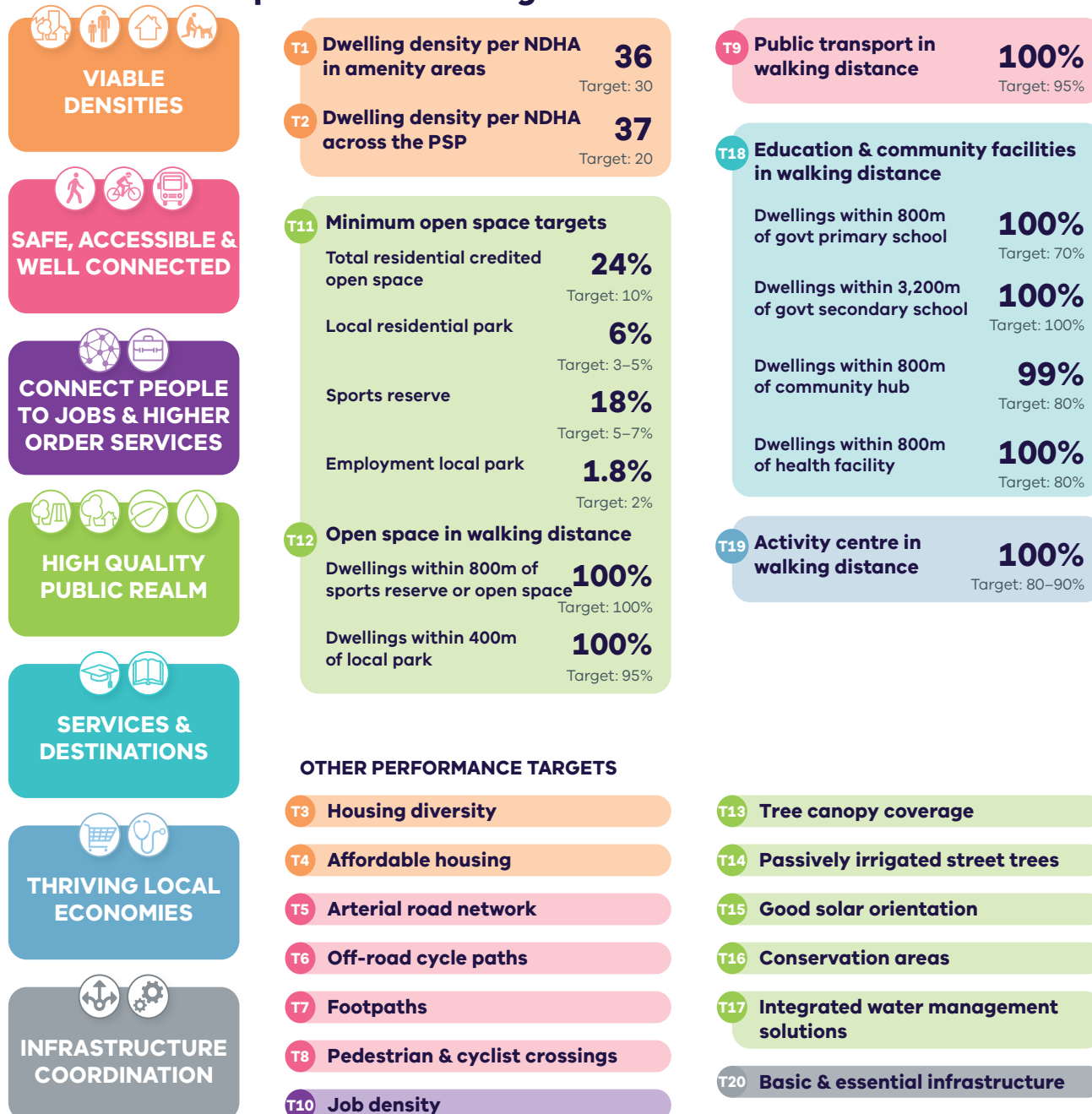


2.3 Precinct snapshot

PSP performance summary



PSP Guidelines performance targets





3.0 IMPLEMENTATION

3.1 Thriving local economies

Plan Melbourne identifies that the Officer–Pakenham State Significant Industrial Precinct (SSIP) is a key industrial precinct in the south-east region. The Officer South (Employment) precinct provides a total of 1,069 hectares of key opportunity for the region and acts as a gateway precinct for the broader Officer–Pakenham SSIP, a key future export and employment centre for the region.

The *Melbourne Industrial and Commercial Land Use Plan* (MICLUP) envisages that the Officer South (Employment) precinct will deliver a SSIP and a RSCA, with no more than 50 per cent of the Regionally Significant Commercial Area (RSCA) to be designated for residential purposes.

State Significant Industrial Precinct

The Officer South (Employment) SSIP provides a key opportunity for the region as a gateway for the broader Officer–Pakenham SSIP.

Key industry opportunities for the precinct include:

- Manufacturing for primary industries (agriculture/mining)
- Manufacturing for knowledge sectors (professional services)
- Manufacturing for health care
- Manufacturing for transport industries (rolling stock and infrastructure)
- Transport and logistics, particularly moving manufactured products
- Wholesaling
- Business-serving professional services.

A future-ready precinct, Officer South (Employment) aspires for innovative technology and smart city opportunities, to not only support a growing economic landscape but become a catalyst for delivering to adaptable new businesses, directly supported by key worker housing and public transport networks for a truly flexible employment environment.

Regionally Significant Commercial Area

The Officer South (Employment) RSCA will provide a significant role in helping to accommodate the commercial demand for the longer term to allow for changing market conditions. This commercial area will provide for an essential buffer between the core industrial activity and neighbouring residential areas of the precinct. The commercial area will evolve over time and will become a beacon of innovation.

Key industry opportunities for the commercial area include:

- Specialised facilities
- Dispersed institutional facilities
- Urban services, office, and retail facilities.

The precinct accommodates land for a proposed emergency services facility and proposed state government facility that may provide a health hub and mental health services to the broader catchment and is envisaged to evolve over time. A potential work hub or industry training institute (centre of excellence) with potential links to surrounding commercial uses, may provide an economic and community enabler with space for business leaders, educators, and residents to meet and share ideas, work, learn and conduct business.

Industrial sub-precincts

Extensive opportunities exist for Officer South (Employment) to lead the vision for an industrial precinct of rich character and place value. Sub-precincts that are arranged to leverage the wealth of the site's offerings will help to guide future development and inspire optimum outcomes. [Figure 1 Sub-precinct concept plan](#) illustrates possible aspirations for the precinct, indicatively demarcating important site to industry relationships that can be drawn upon to achieve sustainable and potentially carbon neutral outcomes for future generation industrial and commercial enterprise.



3.1.1 Place-based objectives – thriving local economies

OBJECTIVES

- O1** To deliver a local town centre of 1 hectare that can accommodate the range of jobs, services, amenities, and activities that support its role and function, and meets the changing economic climate and social needs of Officer South (Employment) precinct.
- O2** To deliver a diverse and vibrant employment precinct to establish the south-east region as a hub for advanced manufacturing and engineering with diversified industrial activities and supporting services.
- O3** To grow a vibrant local, regional, and international food economy which supports growers and enables access to produced food by developing food chains which facilitate growers to participate and operate in their chosen marketplace (e.g. food co-ops, farmers markets and retail spaces).

3.1.2 Requirements and Guidelines – thriving local economies

GUIDELINES

- G1** Cafés, restaurants, convenience shops and convenience restaurants, where possible, should front:
- Drainage channels and waterways
 - Conservation areas, open space or utility reserves
 - Arterial roads or connector streets
- to the satisfaction of the responsible authority.
- G2** Employment sectors are encouraged in commercial and industrial areas at key locations and sub-precincts. Facilities such as the potential industry training institute (Centre of Excellence) are encouraged in commercial / mixed use areas where the sites have high-amenity and are well serviced with access to transport and community facilities.
- G3** Permit applications for section 2 uses within the RSCA and local town centre should have regard to:
- [Table 1 Estimated employment generation](#)
 - [3.1.3 Local town centre delivery guide and composition](#)
 - [A6.1 Local town centre performance requirements and example benchmark outcomes](#)
- and demonstrate how the proposed use contributes to the achievement of the minimum job density and the local town centre design principles in [Appendix 6 Local town centre design criteria](#) or is otherwise a use that supports development of the RSCA and SSIP.

Table 1 Estimated employment generation

LAND USE	NDA (HA)	ESTIMATED JOBS
State Significant Industrial Precinct (SSIP)	522.2	10,236
Regionally Significant Commercial Area (RSCA)	127.1	11,008
Residential (includes local town centre 1 ha & Mixed Use 13.9 ha)	43.8	769
TOTAL	693.16	22,013



3.1.3 Local town centre delivery guide and composition

DELIVERY GUIDE:

The purpose of the Officer South (Employment) local town centre is to service all residents within the precinct and meet their day-to-day retail and community needs. The Officer South (Employment) local town centre will provide for a small grocery store.

COMPOSITION:

Land area:	1 hectare
Retail floor space:	2,300 square metres (including a supermarket not exceeding 1,200sqm)
Commercial floor space:	1,600 square metres



3.2 Connect people to jobs and higher order services

3.2.1 Place-based objectives – connect people to jobs and higher order services

OBJECTIVES

- | | |
|-----------|--|
| O4 | To create a high amenity employment precinct that responds appropriately to the environmental sensitivity of the waterways, conservation and heritage areas, and interface with adjacent residential precincts and rural land, to attract a diversity of businesses and employers and provide jobs to support the Casey–Cardinia Growth area. |
| O5 | To maximise accessibility of the precinct through strengthening links to the Princes Freeway, Thompsons Road and to the surrounding arterial road network, opening key gateways to the Dandenong National Employment and Innovation Cluster (NEIC) and other NEICs, potential future south-east airport and the Port of Hastings to protect the economic prosperity of the precinct and the Greater South East region. |
| O6 | To facilitate access to a quality integrated public transport network that connects people to jobs and higher order services. |
| O7 | To establish a thriving production and distribution precinct with improved and new freight connections that capitalise on arterial roads and the potential future south-east airport. |
| O8 | To provide land for local and regional employment and economic activity and to secure job density. |
| O9 | To foster Workplace Integrated Social Enterprises (WISE) and community initiatives which provide a reliable and affordable source of fresh produce to people experiencing household food insecurity. |

3.2.2 Place-based requirements and guidelines – connect people to jobs and higher order services

REQUIREMENTS

- | | |
|-----------|---|
| R1 | Commercial and industrial development is to be well-integrated through: <ul style="list-style-type: none"> • A road network design that creates a permeable environment between adjoining developable parcels and provides direct connections to arterial roads, and • Building design that positively contributes to the public realm with a strong visual address and active frontages to primary street frontages, delivering a high amenity outcome to the satisfaction of the responsible authority. |
| R2 | Subdivisions should seek to provide a diversity of lot sizes to meet the needs of a range of commercial business and industries, including providing some large lots for large footprint economic and employment uses. |

GUIDELINES

- | | |
|-----------|---|
| G4 | The front setback of commercial and industrial premises should promote an active streetscape and public realm. The front setback should be landscaped with canopy trees where practical and vegetation to promote water sensitive urban design and to help reduce the impact of the urban heat island effect. |
| G5 | Car parks should be landscaped with canopy trees and have adequate pedestrian paths to provide direct, dedicated accessways from parking to building entrances. Large car parks of 6 bays or more should provide a minimum of one tree per six bays or a shade structure that may include solar panels. |
| G6 | Mixed use areas co-located with local parks should build on the surrounding residential uses to accommodate small scale businesses including offices, home based businesses, retail premises and creative enterprise. |

GUIDELINES

G7

Development should consider the inclusion of smart technology to facilitate, support and advance growth as well as encourage ongoing innovation. This may be achieved through:

- Establishment of IoT (Internet of Things) sensor infrastructure to capture environmental and transport data to help monitor air and water quality, heat island effect, micro-climates, climate change resilience and movement of vehicles and people
- Establishment of IoT infrastructure to enhance the community's ability to engage and participate in the digital economy
- Automated lighting to respond to the movement of people and the surrounding environment to improve the safety and vibrancy of private and public spaces
- Wi-Fi stations and digital local community boards for social and wayfinding information.





3.3 Safe, accessible and well-connected

3.3.1 Place-based objectives – Safe, accessible and well-connected

OBJECTIVES

- O10** To design and implement an integrated transport network that facilitates the safe and efficient movement of both people and goods within a local, sub-regional and regional area.
- O11** To create a high amenity, safe, accessible, direct, and suitable walking and cycling environment that allows residents and employees to be active and travel safely and directly between key destinations, local parks, Lower Gum Scrub and Cardinia Creeks.
- O12** To facilitate innovation and alternatives to fossil fuel through delivery of essential supporting infrastructure, including end of trip facilities, solar energy capture, smart city conduit, Information and Communications Technology (ICT) infrastructure, and low emission vehicle infrastructure (e.g. electric vehicle charging station).
- O13** To provide a safe and accessible road layout that identifies and considers bushfire protection measures in the design of the local street network.
- O14** To maximise accessibility for all ages and abilities to employment areas and community facilities, with a particular focus on active and public transport networks.

3.3.2 Place-based requirements and guidelines – safe, accessible and well-connected

REQUIREMENTS

- R3** Road networks and street types must be designed and developed to an urban standard generally in accordance with the cross sections in [Appendix 7 Standard road cross sections & functional layout plans](#) unless otherwise agreed by the relevant authority. Any interim road nominated in [Table 6 Precinct infrastructure](#) as a potential public transport route must be constructed in accordance with the corresponding cross section in the precinct structure plan and the Department of Transport and Planning's guidance for public transport and land use development, to the satisfaction of the responsible authority.
- R4** Any connector road or local access street abutting a community or education facility must be designed to achieve slow vehicle speeds and incorporate a shared path and provide designated pedestrian crossing points, including hard stand areas within the verge, for clear accessibility within the vicinity of a proposed school site and community facility.
- R5** The widths of shared use paths and bicycle paths must be in accordance with current Austroads guidelines, to the satisfaction of the responsible authority.
- R6** Key destinations (active and public open space, local town centre, community facilities and commercial centres) must:
- Be accessible by active and public transport routes
 - Provide active street frontages
 - Provide bicycle parking facilities
 - Include fast charge charging stations for electric vehicles
- To the satisfaction of the responsible authority.

REQUIREMENTS

R7

Design of all subdivisions, streets and roads must provide:

- A permeable, direct, and safe street network prioritising walking and cycling
- Footpaths, at least 1.5 metres wide, on both sides of all streets and roads, except adjacent to community facilities where 1.8 metre footpaths are required to comply with the precinct structure plan and relevant cross section in [Appendix 7 Standard road cross sections & functional layout plans](#)
- Shared paths, bicycle paths and/or alternative transport networks, where identified in the precinct structure plan and relevant cross section in [Appendix 7](#)
- Consistent wayfinding and interpretative signage are created along key shared use paths and cycle paths while raising awareness of the natural values of the precinct
- Safe pedestrian crossings of all roads, local streets, intersections, at key desire lines and destinations and on regular intervals appropriate to the function of the road and public transport provision
- Safe and convenient transition between on- and off-road bicycle networks all to the satisfaction of the relevant road authority and the responsible authority
- Service road, internal loop road or rear lane to lots fronting arterial roads, to the satisfaction of the relevant road authority and the responsible authority
- Convenient access to local and regional points of interest and destinations for effective integration with neighbouring properties, parkland, and sports reserves
- And convenient access to public transport services for walking, cycling and other forms of active transport.

R8

If roundabouts are to be used at major intersections, safe pedestrian and cyclist connections are to be provided with appropriately located pedestrian operated signals to the satisfaction of the coordinating road authority.

R9

Subdivision layouts and the street network must be designed to safely manage stormwater runoff to the satisfaction of Melbourne Water and the responsible authority.

R10

Public pedestrian, potential equestrian paths (where feasible), local roads and infrastructure must be located outside drainage and waterway areas within the conservation areas unless shown on [Plan 5 Public Transport and Active Path Networks](#) and [Plan 13 Utilities](#) or otherwise approved by the relevant authority.

If crossings of the conservation areas are required, they must be planned (and co-located where applicable) to maintain the waterway gully form, drainage function and minimise disturbance to native vegetation and habitat for Growling Grass Frog, Australian Grayling and Dwarf Galaxias habitat and to the satisfaction of Melbourne Water, Department of Transport and Planning, Department of Energy, Environment and Climate Action (DEECA) and the responsible authority. Future waterway crossings cannot fill in or block the waterway gully form.

GUIDELINES

G8

Location and design of walkways or pedestrian and cycle paths in addition to those described through the standard cross sections should consider the need for appropriate lighting and passive surveillance that considers wildlife sensitive lighting principles.

G9

Culs-de-sac should be avoided unless a shared access way to an adjacent road or cul-de-sac is provided. Where culs-de-sac cannot be avoided, they should not negatively impact walkability and cycle connections and provide an appropriate response to crime prevention through environmental design.

G10

The frequency and impact of vehicular crossovers on verges of connector roads should be minimised to avoid undue impacts on public transport movements, using a combination of:

- Rear loaded lots with laneway access
- Vehicular access from the side streets
- Combined or grouped crossovers
- Increased lot widths.



GUIDELINES

G11 Direct off-road active network paths should be delivered between key destination points such as:

- The local town centre including the commercial precinct or local employment areas, local parks, active open space and school destinations
- Access to surrounding precincts, including the Officer Train Station and Town Centre
- Regional destinations.

These active network paths may be located along desired internal pedestrian corridors outside the road reserve where not shown on [Plan 5 Public Transport and Active Path Networks](#) to the satisfaction of the responsible authority.

G12 A variety of connector roads and local access street cross sections should be used in subdivision layouts, as per [Appendix 7 Standard road cross sections & functional layout plans](#), to create differentiation, placemaking and neighbourhood character.

Alternative cross sections should ensure that:

- Minimum required carriageway dimensions are maintained to ensure safe and efficient operation of emergency vehicles on all streets, as well as low line buses on connector streets
- The performance characteristics of standard cross sections as they relate to pedestrian and cycle use are maintained
- Relevant minimum road reserve widths for the type of street are maintained
- Appropriate on-street carparking is provided
- Existing trees identified as 'to be retained' on [Plan 7 Native Vegetation Retention and Removal](#) are retained
- The average 30 per cent tree canopy cover required under [R12](#) can be accommodated (excluding public spaces and areas dedicated to biodiversity or native vegetation conservation)

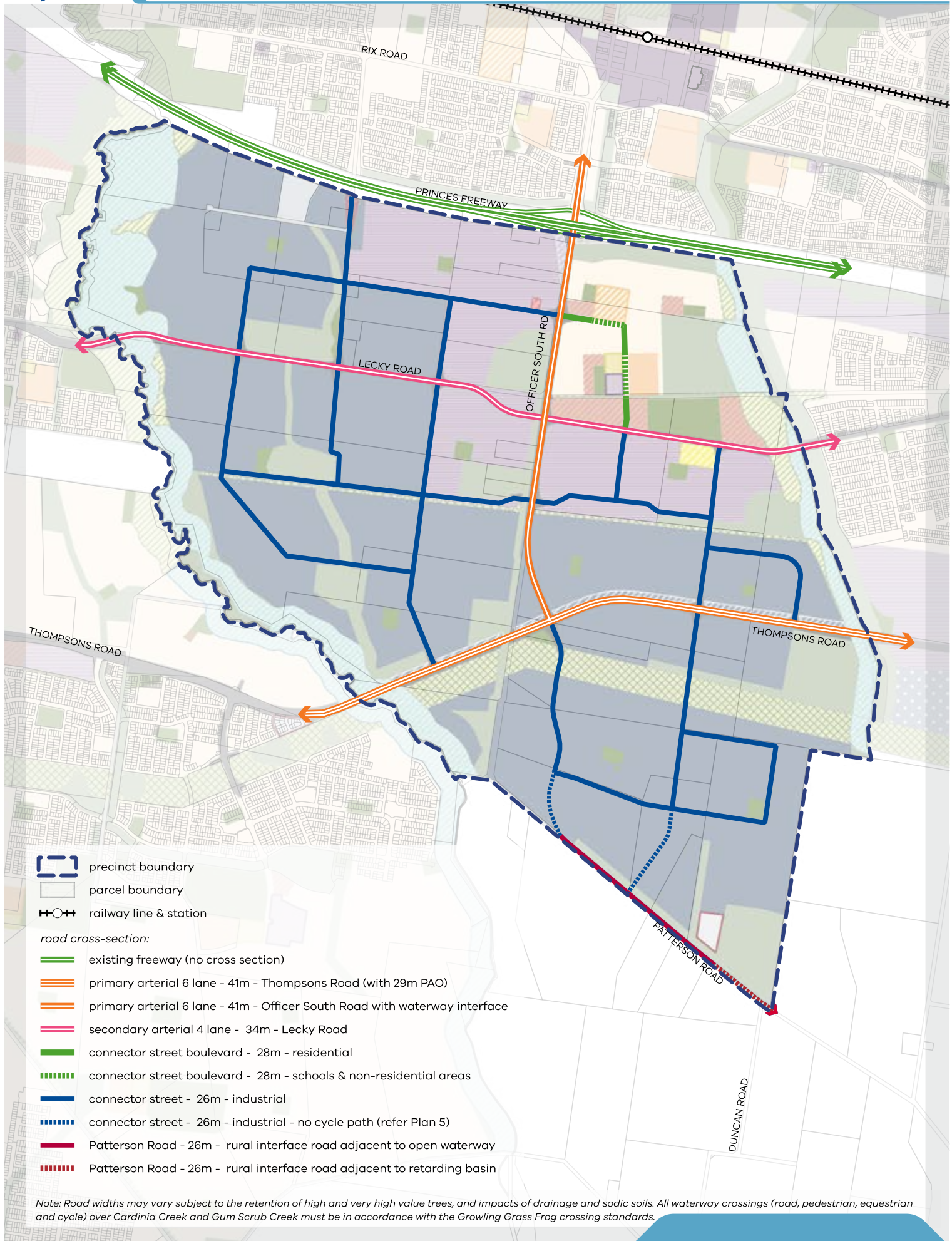
To the satisfaction of the responsible authority.

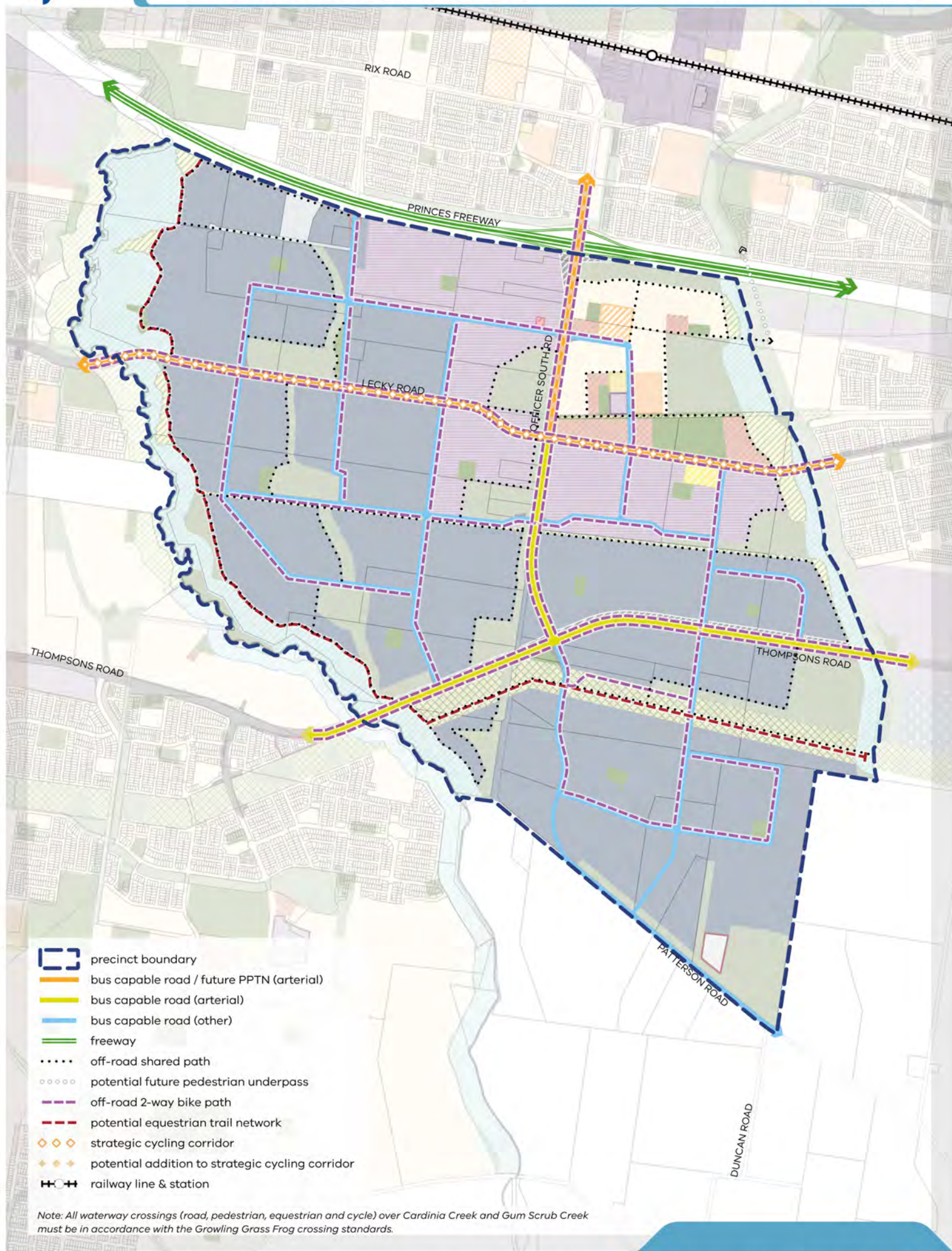
G13 Commercial, industrial and retail premises at-grade parking including loading and storage areas, except for short term /disability parking spaces, should be provided at either the side or rear of the building, with direct pedestrian paths connecting off-street car parks to the primary entrances of buildings to facilitate a high quality and landscaped streetscape. Disability parking spaces should be located close to entrances.

G14 All signalised intersections should be designed having regard to the *Guidance for Planning Road Networks in Growth Areas* (November 2015) and be to the satisfaction of the Head, Transport for Victoria.

G15 Minimise the number of utility and road crossings, including local roads, over the high-pressure gas easement within the precinct to align with [Plan 3 Place Based Plan](#) and [Figure 8 Gas Easement Concept](#).









3.4 High quality public realm

3.4.1 Place-based objectives – high quality public realm

OBJECTIVES

- O15** To support the protection and conservation of cultural heritage, Traditional Owner values, and post-contact heritage values.
- O16** To deliver an integrated and linked network of open space that provides equitable access for residents and workers to amenities and facilities for sport and recreation, including environmental, cultural and visual amenity along natural and constructed waterways, gas and electrical easements, streets, parks and public spaces, in alignment with Cardinia Shire Council's *Open Space Strategy*.
- O17** To facilitate safe, resilient, water sensitive, environmentally sustainable urban development and urban forestry that responds to climate change and other hazards.
- O18** To protect and enhance areas of biodiversity and native vegetation value (local, state, nationally significant) including the biolinks and BCS conservation areas along Cardinia Creek and Lower Gum Scrub Creek.
- O19** To develop sustainable water, drainage and wastewater systems that protect, conserve and improve biodiversity, waterways and other natural resources and maintains or enhances the safety, health and wellbeing of people and property.
- O20** To deliver an integrated water management system that reduces reliance on reticulated potable water, increases the re-use of alternative water, responds to local soil types, minimises flood risk, ensures waterway health, and contributes towards a sustainable and green urban environment.
- O21** To provide high quality interfaces between development, waterway and drainage assets to maximise community use and enjoyment of these places.
- O22** To create a sustainable urban landscape focused on achieving net zero carbon emissions, which enhances the existing biodiversity, cultural heritage, drainage, and landscape values within the precinct and mitigates urban heat island effect.
- O23** To plan for the long-term conservation of flora and fauna species through the protection of habitat and biolinks across the precinct, particularly habitat for Growling Grass Frog and Australian Grayling and Dwarf Galaxias in the BCS conservation areas along Cardinia Creek and Lower Gum Scrub Creek.

3.4.2 Place-based requirements and guidelines – high quality public realm

REQUIREMENTS

- R11** Trees and vegetation in streets, civic places and the passive open space network must be provided in accordance with the Cardinia Shire Council *Developer Landscape Guidelines 2017* (or as amended), and must:
- Comprise a mix of native flowering and non-flowering species and include either native, indigenous and exotic species and any other species as appropriate to the location, design and local conditions. Edible planting (e.g. fruits, nuts, herbs and bush foods) are encouraged in open space areas (not within streetscapes)
 - Be larger species to facilitate continuous canopy cover
 - Be planted in modified and improved soil to support tree establishment
 - Be appropriately sized to nature strips, nearby utilities, and buildings.
- R12** Unless provided with the application, a condition of any permit issued for the subdivision of land within the PSP area must require a landscape plan to be submitted to the responsible authority for approval. The landscape plan must illustrate and quantify canopy tree coverage within the public realm and open space to an average of 30 per cent (excluding areas dedicated to biodiversity or native vegetation conservation).

REQUIREMENTS

R13 Development and subdivision must provide for a minimum 19 metre perimeter road or alternative bushfire interface at the conservation area boundary, drainage reserves, constructed waterways, and the southern and south-east precinct boundary adjoining the Green Wedge. The bushfire interface area may incorporate paths, open space, drainage, flood protection, and stormwater quality treatment infrastructure.

Where a setback from a bushfire hazard area is required by [Plan 8 Bushfire Hazard Areas](#) and/or [Table 3 Bushfire hazard vegetation management and setback requirements](#), vegetation within the setback must be managed as follows, unless otherwise agreed by the responsible authority and relevant fire authority:

- Grass must be short cropped and maintained during the declared fire danger period
- All leaves and vegetation debris must be removed at regular intervals during the declared fire danger period
- Shrubs must not be located under the canopy of trees
- Individual and clumps of shrubs must not exceed five square metres in area and must be separated by at least five metres
- The canopy of trees must be separated by at least two metres at maturity
- There must be a clearance of at least two metres between the lowest tree branches and ground level
- Within 10 metres of a building, flammable objects must not be located close to the vulnerable parts of the building
- Plants greater than 10 centimetres in height must not be placed within three metres of a window or glass feature of the building.

Consultation is required with Department of Energy, Environment and Climate Action Melbourne Strategic Assessment (DEECA MSA), Country Fire Authority (CFA), Melbourne Water and council to confirm the conservation area planting and bushfire risk implications prior to undertaking a development application.

R14 Pre- and post-contact cultural heritage and history must be recognised through the design of public places and infrastructure.

R15 Passive open space, roadways, pathways and other infrastructure must be located to protect any tree with Aboriginal cultural significance which is identified for protection through an approved Cultural Heritage Management Plan (CHMP).

R16 In consultation with the appointed Registered Aboriginal Party (RAP), signage or opportunities for cultural celebration and interpretation must be explored and must be integrated into the public realm, utilised along potential equestrian trails and walking paths at conservation areas, to support and contribute to the knowledge and understanding of the local Aboriginal cultural heritage and values.

R17 A mandatory CHMP is required to be undertaken prior to development and/or subdivision occurring within the cultural value area as identified in [Plan 3 Place Based Plan](#), as required by the *Aboriginal Heritage Act 2006*. Interface outcomes with culturally sensitive sites must be delivered in conjunction with the CHMP and in consultation with the appointed RAP.

R18 Reinstatement of creeks, waterways, water bodies and other water features must be undertaken with indigenous species, to the satisfaction of the relevant manager of the creeks, waterways, water bodies and other water features, in accordance with a landscape plan prepared to the satisfaction of the responsible authority and taking into account any comments from Melbourne Water and the appointed RAP.

R19 Development impacting Victorian Heritage Inventory site H7921-0129 must obtain a *Heritage Act 2017* Consent to authorise any works that may affect historical archaeological remains at the place.

R20 Existing high and very high value trees identified on [Plan 2 Precinct Features](#) and [Plan 7 Native Vegetation Retention and Removal](#) must be retained and incorporated into the public realm (including road reserves, biolinks, and public open space) where possible to the satisfaction of the responsible authority.



REQUIREMENTS

R21 Stormwater runoff from the development must meet the performance objectives of the *Commonwealth Scientific and Industrial Research Organisation Best Practice Environmental Management Guidelines for Urban Stormwater* (or as amended) and the relevant Development Services Scheme(s) prior to discharge to receiving waterways and as outlined in [Plan 9 Integrated Water Management](#), unless otherwise approved by Melbourne Water and the responsible authority.

Proposals that exceed the performance objectives are highly encouraged and can be considered, all to the satisfaction of South East Water, Melbourne Water, and the responsible authority.

R22 Permit applications for subdivision and/or development must be accompanied by an appropriate integrated water management plan which clearly identifies how development will contribute towards [Plan 9 Integrated Water Management](#) and towards:

- Outcomes applicable to the development identified in the DEECA *Western Port Strategic Directions Statement* (September 2018) and *Western Port IWM Catchment Scale Plan* (September 2022)
- Potable water reduction targets applicable to the development as outlined in the *Greater Melbourne Urban Water and System Strategy*
- Waterways and integrated water management outcomes which enable land to be used for multiple recreation and environmental purposes
- Supply and use of recycled water for residential and public realm purposes, and if agreed by South East Water, for industrial and commercial land
- The delivery of the applicable Melbourne Water approved Development Services Scheme(s)

all to the satisfaction of South East Water, Melbourne Water, and the responsible authority.

R23 The final layout and design of constructed wetlands, retarding basins, constructed waterways, stormwater infrastructure and associated paths, boardwalks, bridges and planting, must be designed to the satisfaction of Melbourne Water and the responsible authority. Changes may be made to the size or location of drainage assets, including wetlands, retarding basins or constructed waterways, with the approval of Melbourne Water. Any such changes will be 'generally in accordance with' the PSP if the asset achieves the intended performance standards to the satisfaction of Melbourne Water.

R24 Waterways must:

- Provide safe drainage and flood protection
- Incorporate environmental, cultural and amenity value
- Provide open waterways

all to the satisfaction of Melbourne Water and the responsible authority.

R25 For development draining into Lower Gum Scrub Creek, development proposals must also demonstrate the management of stormwater flow and volume to ensure no additional downstream inundation impacts, duration impacts and flood impacts are caused by the development of land, to the satisfaction of Melbourne Water and the responsible authority.

R26 Where development is proposed in areas adjacent to, but outside of the waterway corridor/conservation area along Cardinia Creek, development must demonstrate to the relevant authority that this area can be serviced for drainage and is consistent with the relevant Development Services Scheme(s). Development must not have any adverse impact on flood protection, drainage and waterway health along Cardinia Creek.



REQUIREMENTS

R27 Drainage, flood protection, and stormwater quality treatment infrastructure must be designed generally in accordance with [Plan 9 Integrated Water Management](#), and to implement the relevant Development Services Scheme(s) (subject to any necessary revision to respond to cultural heritage or environmental values) to avoid or mitigate the risk of erosion of sodic and/or dispersive soils, and to appropriately manage flow rate and flow event volume, to the satisfaction of Melbourne Water and the responsible authority.

Note: the response to further assessment of values within the cultural value area, and/or environmental values, may result in variation to the Melbourne Water assets as shown on [Plan 9 Integrated Water Management](#).

R28 Where local parks and recreation areas occur adjacent to BCS conservation areas, they must be designed to complement the outcomes required in Section 5 of the BCS relating to Conservation Area 36 and allow for bushfire buffer requirements outside the conservation area, to the satisfaction of the Secretary, DEECA.

R29 Any proposed development or works within a BCS conservation area must obtain the approval of DEECA, and must be generally in accordance with the corresponding conservation area concept plans in [Appendix 2 Conservation area concept plans](#) and cross sections in [Appendix 7 Standard road cross sections & functional layout plans](#). No planting of tall shading trees is permitted where Growling Grass Frog wetland Areas of Strategic Importance is adjacent to the conservation area boundary.

R30 Public lighting must be designed to the lowest AS/NZS 1158 lighting category that can be reasonably applied, use the warmest colour temperature possible (less 3000K), and be baffled to prevent light spill and glare within and adjacent to the BCS conservation area, unless otherwise agreed by DEECA.

R31 Development in or adjacent to the BCS conservation area must:

- Ensure suitable hydrological patterns necessary for the ecological requirements of Matters of National Environmental Significance (MNES) are maintained in the conservation areas as far as practical
- Maintain passage for fauna species including Growling Grass Frog, Australian Grayling and Dwarf Galaxias except where online wetland/retarding basins are required in Gum Scrub Creek
- Minimise utility corridors intercepting the conservation area
- Ensure there are no significant impacts from overshadowing to Growling Grass Frog wetlands (existing and proposed) or native vegetation patches
- Mitigate flood flows and volumes into the receiving waterways
- Be consistent with the cross sections in [Appendix 7 Standard road cross sections & functional layout plans](#).

R32 Any infrastructure crossing Cardinia Creek and Lower Gum Scrub Creek must be designed to avoid impacting native vegetation within the BCS conservation area to the greatest extent practicable and must be designed and constructed in accordance with the *Growling Grass Frog Crossing Design Standards* (2017) to the satisfaction of DEECA, Melbourne Water and the responsible authority.

R33 Drainage from stormwater treatment infrastructure must be supported by an ecology report that demonstrates that when the ultimate drainage works are in place, the works will have no significant impact on habitat for Matters of State and National Environmental Significance within BCS conservation areas, to the satisfaction of the responsible authority and DEECA.

R34 The BCS conservation area boundary must be fenced to exclude vehicles, allowing for pedestrian and maintenance access where required, to the satisfaction of DEECA and the responsible authority.

R35 The vegetation within the bushfire hazard areas shown on [Plan 8 Bushfire Hazard Areas](#), must be managed to a level that ensures it does not result in a vegetation class that would require a greater 'separation distance' than that specified in [Plan 8 Bushfire Hazard Areas](#).



REQUIREMENTS

- R36** The final layout and design of constructed wetlands, retarding basins, and stormwater quality treatment infrastructure adjacent to BCS conservation areas must consider:
- The potential to supply treated stormwater for Growling Grass Frog wetlands
 - Hydraulic and hydrological requirements to ensure habitat protection and ecological requirements of Dwarf Galaxias and Australian Grayling within Cardinia Creek as identified in:
 - *Cardinia Creek Hydrological and Fish Risk Assessment* (Jacobs, for Melbourne Water, 2021)
 - *Cardinia Creek Fish Assessment Amendment* (Jacobs, for Melbourne Water, 2023)
- to the satisfaction of Melbourne Water and the Secretary, DEECA.
-
- R37** Subdivision and development surrounding or relating to site HO92 at 425 Officer South Road must ensure:
- The 'original gate' from the Patterson era located in the property, identified in Plate 4 shown on page 33 of the *Officer South (Employment) PSP – Post-contact Heritage Assessment* (BHM 2022) is restored and erected within the Heritage Overlay boundary
 - A Heritage Interpretation Plan be prepared to ensure an interpretation outcome which would address the whole of the site and its historical and social significance to the local and regional context.
-
- R38** Planning of development within the precinct must:
- Aim to integrate with the BCS conservation area in a manner that will not degrade the habitat values, such as by introducing weeds, pest animals, contaminants, and will not result in major alterations to the hydrology other than in accordance with the applicable Development Services Scheme(s)
 - Consider any effects that development may cause downstream on the Western Port Ramsar wetland
 - Minimise sediment run off and contaminant loads in accordance with EPA Guidelines and the applicable Development Services Scheme(s). This may require some developments to store and treat stormwater on site and have appropriate controls for contaminant spills or leaks.
-
- R39** Development must consider locating service lanes or roads to separate development from bushfire hazard areas on [Plan 8 Bushfire Hazard Areas](#), including the Princes Freeway, to the satisfaction of the responsible authority.
-
- R40** Development must provide a lot-based rainwater tank where recycled water is not available.
-

GUIDELINES

- G16** Subdivision and development applications should deliver an open space network that:
- Responds to the Cardinia Shire Council Developer Landscape Guidelines Policy 2017 (or as amended)
 - Considers the Cardinia Shire Council *Recreation Reserve Facility Standards Policy 2019* (or as amended)
 - Includes a range of open space shapes, typologies, and classifications
 - Maximises the amenity and value of service to open space through the provision of paths, trails, and other recreational elements
 - Responds to the values of any adjoining open space, waterways, drainage reserves, trees, conservation areas, and Aboriginal and post-contact heritage
 - Provides flexible recreational opportunities that allow for urban agriculture uses required by the community, where appropriate
 - Provides space allocation for future energy infrastructure where appropriate
- to the satisfaction of the responsible authority.
-
- G17** Where a canopy tree cannot be provided because of local climate and soil conditions, a suitable species may be selected which closest achieves this diameter. The requirement in [R12](#) for a minimum average of 30 per cent canopy tree coverage within the public realm and open space should still be met (excluding areas dedicated to biodiversity or native vegetation conservation).
-

GUIDELINES

- G18** Open spaces should have a frontage road to all edges except when abutting mixed use development or drainage reserve, or where housing fronts open space with an access way, to the satisfaction of the responsible authority. Direct frontage and rear access ways are not an acceptable interface to the BCS conservation area where a 30 metre conservation interface is shown on Figures 3–7 in [Appendix 2 Conservation area concept plans](#).
- G19** A consistent suite of lighting and furniture should be used across residential neighbourhoods and employment areas, appropriate to the type and role of public space, to the satisfaction of the responsible authority.
- G20** Where developed, linear parks should be located and designed to:
- Enhance the wider walking and cycling network
 - Connect and integrate key neighbourhood destinations and landscape features
 - Support residential and employment neighbourhood legibility and sense of place
 - Enhance the diversity of open space environments and access to urban agriculture
 - Provide active frontages
 - Avoid vehicle crossings
 - Assist in achieving higher residential densities
- to the satisfaction of the responsible authority.
- G21** Where possible, salvaged rocks should be incorporated into the design of waterways, retaining structures, fences, and other landscape features.
- G22** Significant landscape features, such as high points, vegetation, open space, and waterways should be used as focal points for view lines along streets.
- G23** Subdivision design should incorporate natural and constructed design elements which respond to local heritage, neighbouring land uses and topography to assist in place making and the achievement of a 'sense of place'.
- G24** Subdivision design should respond sensitively to the visual setting and character of pre- and post-contact heritage places and values by providing appropriate distance to new industrial buildings through large lot configurations at these sites.
- G25** Subdivision and development surrounding or relating to site HO92 at 425 Officer South Road:
- Should consider providing an open space buffer/public reserve between the boundaries of the Heritage Overlay and any future development
 - Should not reduce the extent of the garden area surrounding the house as it is presently defined within the Heritage Overlay boundary.
- G26** Subdivision and development surrounding site HO_91 at 410 Officer South Road should consider providing an open space buffer/ public reserve between the northern and western boundaries of the garden. The layout and planting of the open space should aim to preserve the views from the Cardinia Creek homestead to the east. There should be no reduction in the extent of the garden area surrounding the house as it is presently defined.
- G27** Prior to demolition of the dairy at 185 Officer South Road, the contents of the dairy, namely the intact brick and render water trough and the tin feed room should be recorded in detail and provided to the Berwick-Pakenham Historical Society along with the contents of the dairy.
- G28** Existing vegetation and trees should be retained, protected, and enhanced to provide habitat and movement corridors for local fauna. Where existing vegetation and trees are located along a street, roads may be realigned or reserve widths may be varied to ensure that the provision of footpaths, utility services, and drainage does not compromise the long-term health of the vegetation, to the satisfaction of the responsible authority.

GUIDELINES

- G29** Development and subdivision should:
- Protect and manage Matters of State and National Environmental Significance, particularly within conservation areas, in relation to water quality and suitable hydrological regimes (both surface and groundwater)
 - Enable supply of gravity fed treated stormwater for existing and future Growling Grass Frog wetlands where possible
 - Maximise habitat values for local flora and fauna species support Flood Sensitive Planning and Urban Design (FSPUD) outcomes
 - Recognise and respond to Aboriginal cultural heritage significance where identified through a CHMP.
-
- G30** Where a CHMP is required, this should provide recommendations for the ongoing management and maintenance of any creeks, wetlands and water features that are found to have cultural heritage value. Any such ongoing management and maintenance requirements (in respect of land that will remain in private ownership) should be considered to be included as an appropriately worded condition on a relevant planning permit. Where possible, management and maintenance requirements should be consistent for the length of the cultural value area.
-
- G31** Drainage assets and public open space areas should be located adjacent to the conservation area and waterways to provide a buffer to development where appropriate, to the satisfaction of the responsible authority, and subject to the requirements of any approved CHMP.
-
- G32** Development proposals should enhance habitat for native flora and fauna, providing opportunities for people to experience nature in urban areas. This should demonstrate the principles of Biodiversity Sensitive Urban Design (BSUD) to the satisfaction of the responsible authority that:
- Protects and creates habitat
 - Helps species disperse
 - Encourages positive human-nature interaction.
-
- G33** The design and layout of public open space within commercial and residential areas, community facilities, schools, transmission easements and sporting reserves should consider space for growing, harvesting, distributing, and consuming food where appropriate. This should be achieved using Flood Sensitive Planning and Urban Design (FSPUD) initiatives such as urban farming, raised garden beds, car space gardens, rooftop gardens and community gardens that utilise rainwater or recycled water, as negotiated with South East Water and to the satisfaction of the responsible authority.
-
- G34** Applications should consider a range of IWM options where practicable and where agreements are in place with drainage authorities for future management as indicated in [Plan 9 Integrated Water Management](#). Potential IWM options may include but are not limited to:
- Rainwater tanks on all industrial, commercial, and residential lots
 - Passive irrigation of trees in the public realm, including all streets and public open space
 - Providing local stormwater harvesting systems and supplying stormwater to service the watering needs of future open spaces
 - Regional scale rainwater tanks collecting roof water from all industrial, commercial and residential lots to be reticulated back within the precinct
 - Supplying stormwater of the right quality to existing and future Growling Grass Frog wetlands
 - Use of Melbourne Water stormwater management assets to provide green corridors and community amenity
 - Identifying designs to maximise opportunities for infiltration
 - Regional scale stormwater harvesting systems to service the watering needs of the precinct and beyond.
-
- G35** Development and subdivision should:
- Avoid and minimise impacts to conservation areas, including with regard to the location of essential and other services
 - Provide appropriately managed and sensitively designed community access to conservation and landscape values areas and ensure protection of the primary conservation and landscape values can be maintained.
-



GUIDELINES

G36 The design and layout of roads, road reserves, and public open space should optimise water use efficiency and long-term viability of vegetation, tree canopy and public uses to contribute to a sustainable and green urban environment. This should be achieved through the use of overland flow paths, passive watering and Water Sensitive Urban Design initiatives such as street swales, rain gardens and/or locally treated storm water for irrigation.

G37 All local parks should be located, designed and developed generally in accordance with the relevant description in [Table 2 Credited open space delivery](#) and any local open space strategy, to the satisfaction of the responsible authority.

An alternative provision of land for local parks to that illustrated on [Plan 6 Open Space and Community Facilities](#) is considered to be generally in accordance with this plan provided the local park:

- Is located so as to not reduce the walkable access to local parks demonstrated on [Plan 6 Open Space and Community Facilities](#)
- Does not diminish the quality of usability of the space for passive recreation
- Is located so as to ensure no heritage buildings are located within local parks
- Is equal to or more than the passive open space provision within the Officer South (Employment) ICP.

G38 On land where cultural heritage is identified, naming opportunities for neighbourhoods, roads, streets, parks, wetlands or conservation zones, passive or active open spaces and community or local town centres should be given to Traditional Owners, represented by the appointed RAP. This should include consultation with Traditional Owners, represented by the appointed RAP to inform the potential naming opportunities.

G39 Potential management methods to avoid or mitigate the risk of erosion of sodic and/or dispersive soils may include but are not limited to:

- Widening the buffer distances between the core riparian zone and the outside vegetated buffers that allows sufficient tolerances for channel migration
- Diversion of water away from sodic and/or dispersive materials
- Minimising potential convergence and/or ponding of surface flows
- Compacting to reduce pore spaces and minimise water movement through material
- Physical and chemical soil ameliorants
- Maintenance of topsoil across undisturbed land, preferably with grasses to provide surface soil stability and root anchorage
- Minimising the amount of time land is exposed (e.g. by staging development)
- Ensuring that culverts and drains excavated into dispersive subsoils are capped with non-dispersive topsoil, gypsum stabilised and vegetated.



Table 2 Credited open space delivery

PARK ID	AREA (HA)	TYPE	LOCATIONAL ATTRIBUTES	RESPONSIBILITY
ACTIVE OPEN SPACE				
SR-01	7.82	Local Sports Reserve	Located in the north of the precinct to serve the Officer South (Employment) and Cardinia Road (Employment) residential areas, and broader catchment via the street network.	Cardinia Shire Council
LOCAL PARKS				
LP-01	0.77	Local Park	Located to provide passive open space to the industrial area.	Cardinia Shire Council
LP-02	0.77	Local Park	Located to provide passive open space to the commercial area.	Cardinia Shire Council
LP-03	0.75	Local Park	Located to provide passive open space to the residential area.	Cardinia Shire Council
LP-04	0.75	Local Park	Located to provide passive open space to the residential area.	Cardinia Shire Council
LP-05	1.00	Local Park	Located to provide passive open space in the industrial area next to the conservation area	Cardinia Shire Council
LP-06	0.98	Local Park	Located to provide passive open space in the industrial area next to a drainage reserve	Cardinia Shire Council
LP-07	1.00	Local Park	Located to provide passive open space next to the local town centre	Cardinia Shire Council
LP-08	1.00	Local Park	Located to provide passive open space in the industrial area next to a drainage reserve	Cardinia Shire Council
LP-09	1.00	Local Park	Located to provide passive open space in the commercial area next to a drainage reserve	Cardinia Shire Council
LP-10	1.00	Local Park	Located to provide passive open space in the commercial area	Cardinia Shire Council
LP-11	0.70	Local Park	Located to provide passive open space to the industrial area	Cardinia Shire Council
LP-12	0.50	Local Park	Located to provide passive open space to the industrial area	Cardinia Shire Council
LP-13	1.00	Local Park	Located to provide passive open space in the industrial area next to the Heritage Overlay	Cardinia Shire Council



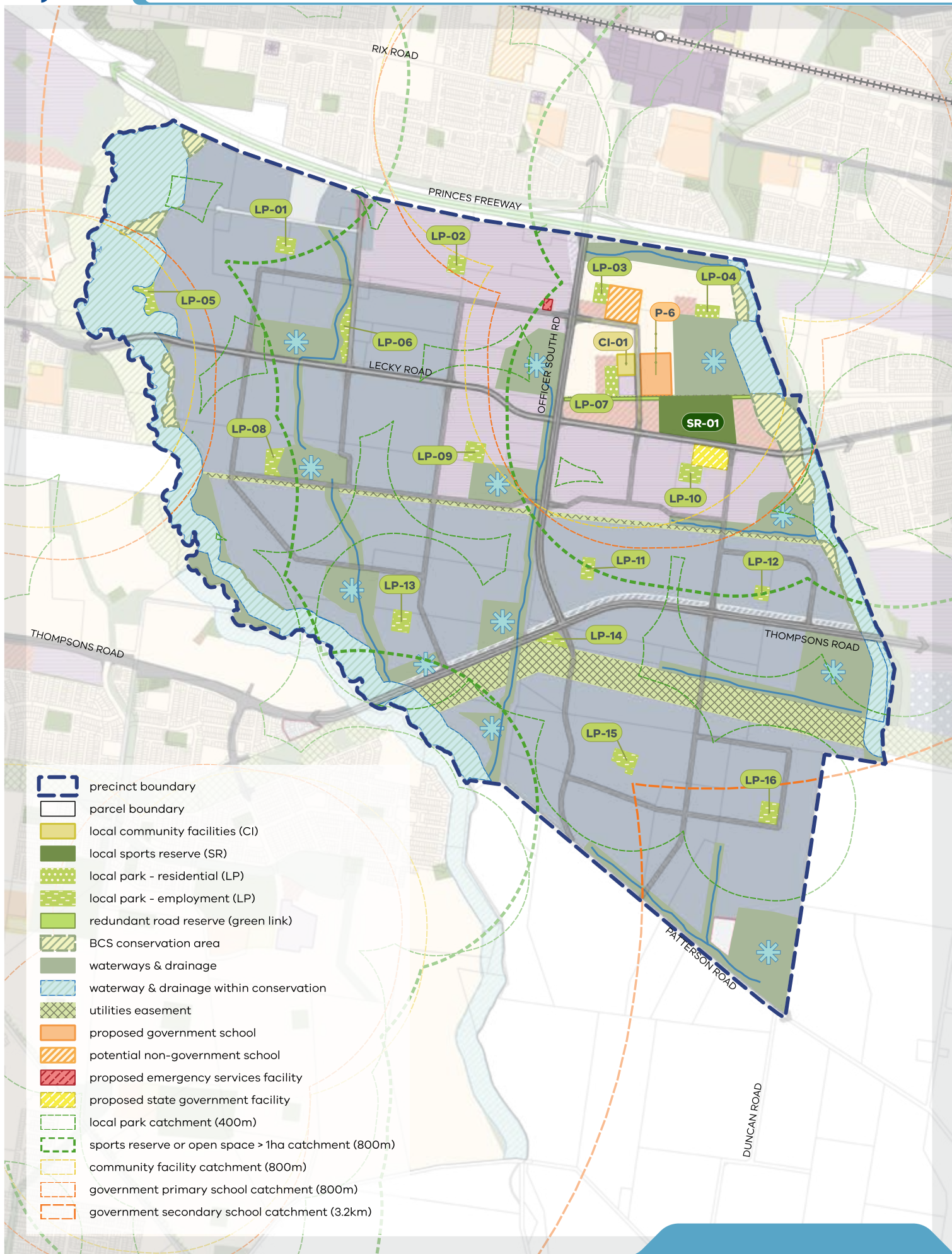
PARK ID	AREA (HA)	TYPE	LOCATIONAL ATTRIBUTES	RESPONSIBILITY
LP-14	0.67	Local Park	Located to provide passive open space along the high-pressure gas easement in the industrial area.	Cardinia Shire Council
LP-15	1.00	Local Park	Located to provide passive open space next to the Heritage Overlay.	Cardinia Shire Council
LP-16	1.00	Local Park	Located to provide passive open space to the industrial area.	Cardinia Shire Council

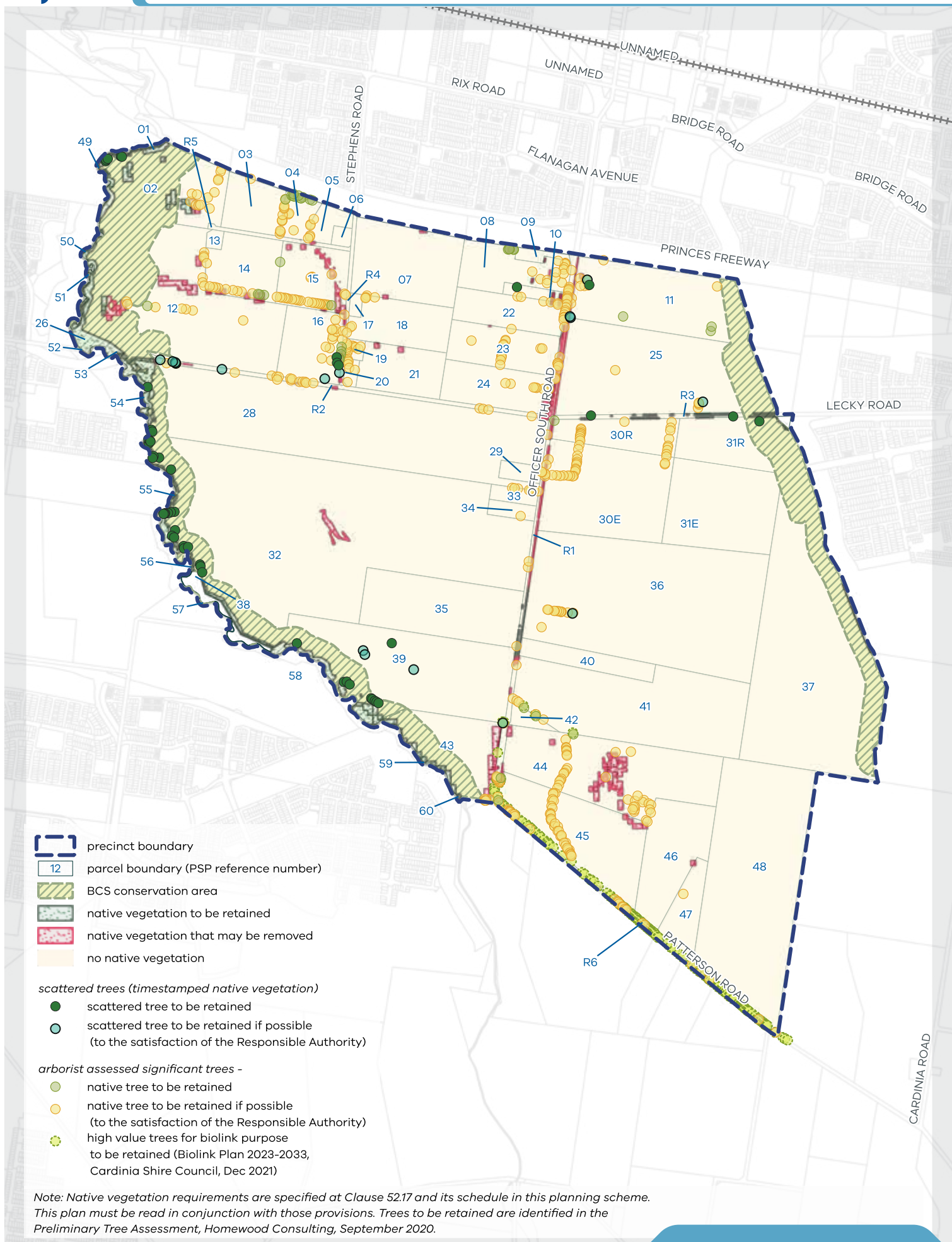
Note: The total area for SR-01 is 8.5 hectares, including 0.68 hectares of green spine land.

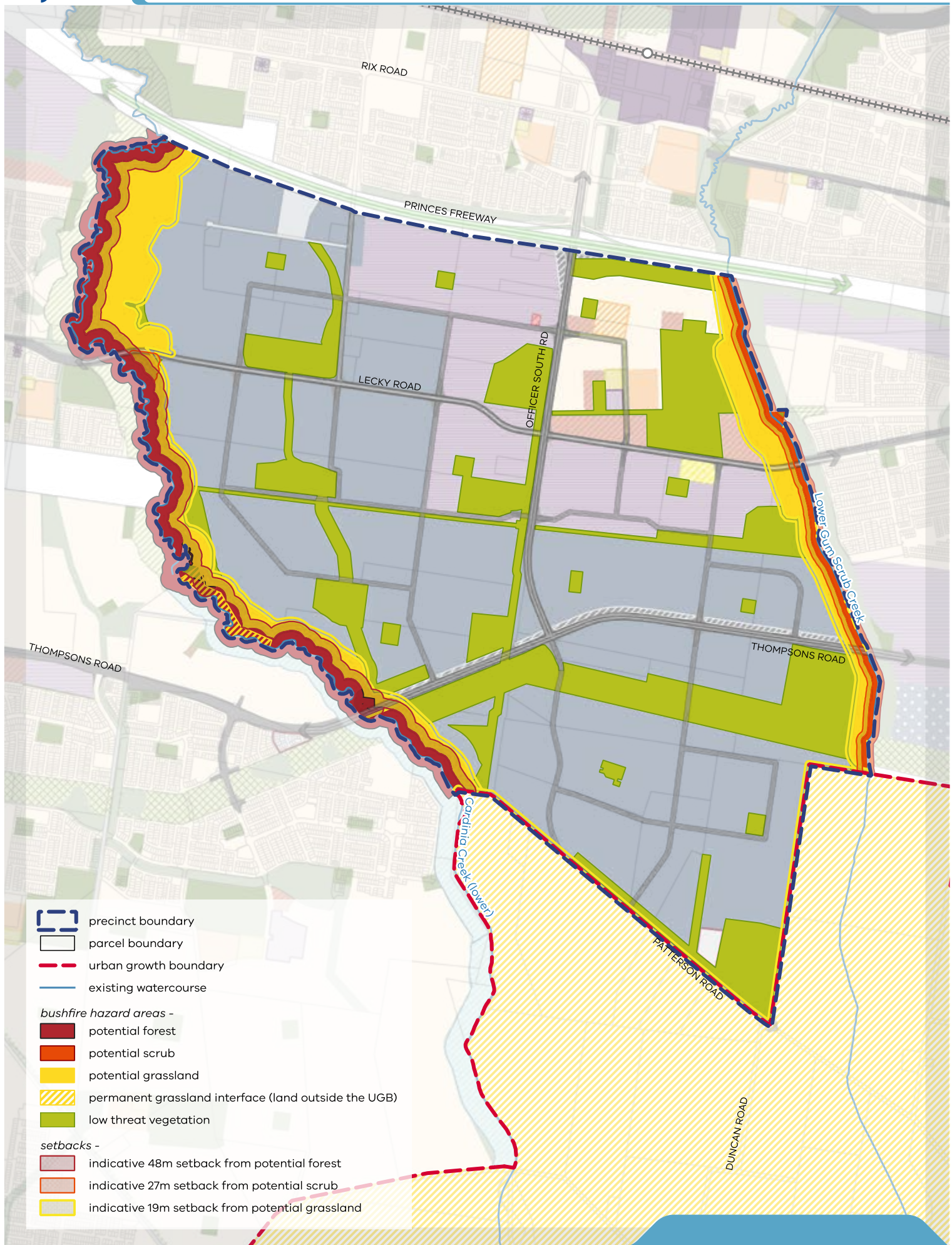
Note: LP-13 and LP-15 are to be located to ensure no heritage buildings are located within local parks.

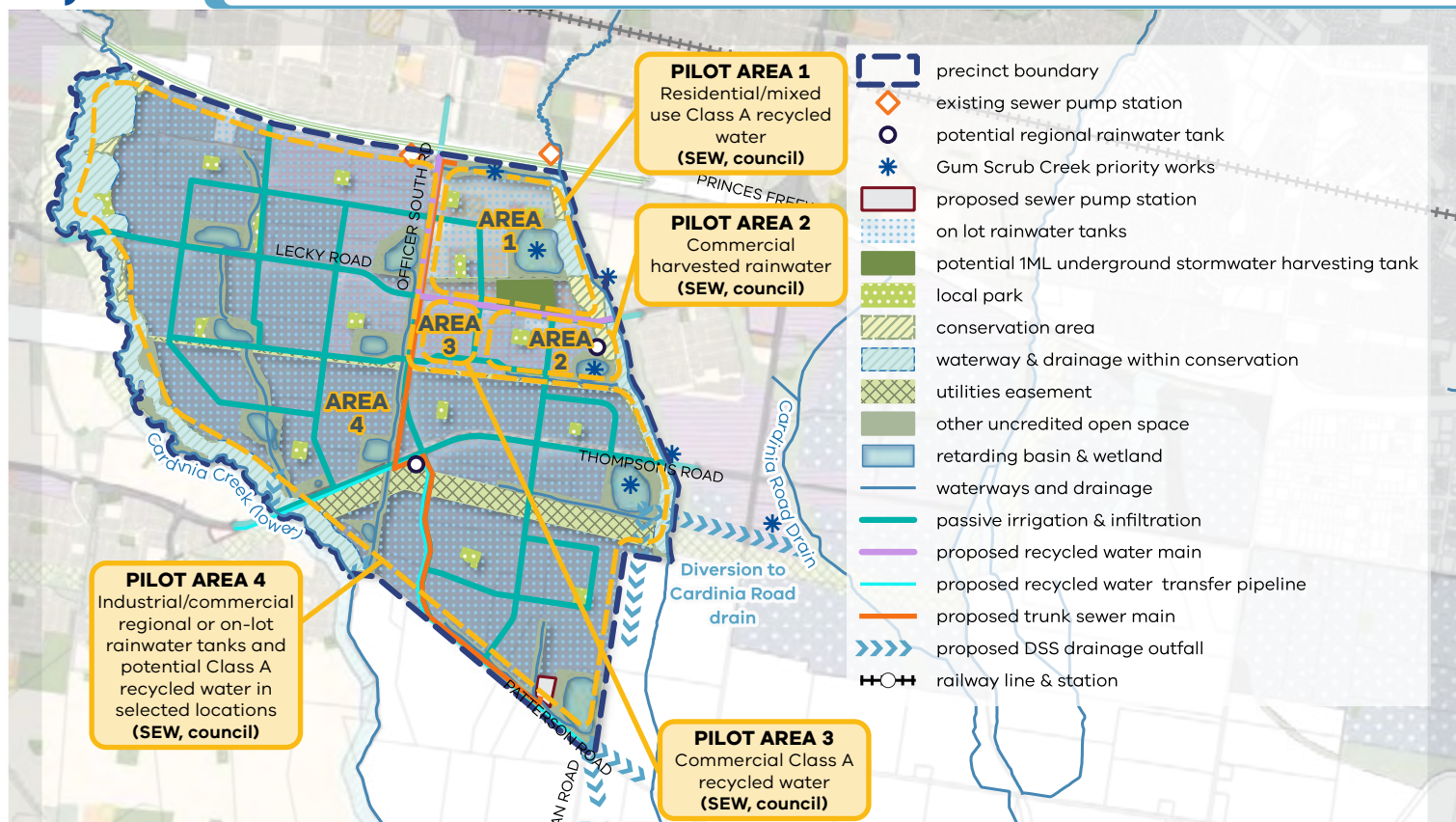
Table 3 Bushfire hazard vegetation management and setback requirements

SLOPE CLASS	VEGETATION	SETBACK DISTANCE IN ACCORDANCE WITH AS3959 – 2018 (OR AS AMENDED)
All upslopes and flat land	Grassland and Shrubland	19 m
	Scrub	27 m
	Woodland	33 m
	Forest	48 m









IWM opportunities identified for Officer South (Employment) PSP can contribute to the seven strategic outcomes in the *Western Port IWM Forum Strategic Directions Statement* (see below).

Drainage assets may be refined through detailed design, including size and/or location, to the satisfaction of Melbourne Water and the responsible authority. Changes to the size or location of assets will be 'generally in accordance with' the PSP if the asset achieves the intended performance standards.

The IWM opportunities shown on this plan are suggested in addition to the infrastructure required under the DSS. IWM for each proposal is to be considered on a site-by-site basis in consultation with SEW, Melbourne Water and DEECA to determine implementation, operation and maintenance requirements (relevant stakeholders are provided in brackets).

Drainage assets along Cardinia Creek may be relocated to within the Cultural Value Area subject to Cultural Heritage Management Plans and at the election of the developer.



Jobs, economic growth and innovation

- Water to support jobs and economic growth (council, SEW)



Safe, secure, and affordable supplies in an uncertain future

- Lot-scale rainwater and stormwater harvesting substitution for potable water demands (council, SEW)
- Regional scale rainwater and stormwater harvesting (MW, council, SEW)
- Stormwater for alternative use – storage and transfer infrastructure (MW, SEW)



Opportunities are sought to manage existing and future flood risks and impacts

- Flood mitigation via the Melbourne Water Development Services Scheme (MW)



Community values are reflected in place-based planning

- Community engagement for IWM initiatives (council, SEW)



Effective and affordable wastewater systems

- Sewer pumping station and raising main (SEW)
- Recycled water for residential and employment uses (SEW)



Healthy and valued urban, rural, agricultural, and green landscapes

- Passive irrigation of street trees to support tree health and achieve minimum tree canopy coverage targets (council)
- Stormwater harvesting for passive and active open space irrigation (council)
- Drainage reserves and conservation areas provide green corridors for increased amenity (MW, council, DEECA)



Healthy and valued waterways and marine environments

- Stormwater infiltration via Water Sensitive Urban Design (council, MW)
- Stormwater supply to Growling Grass Frog conservation areas (MW, DEECA)



3.5 Viable densities

The Officer South (Employment) residential precinct is a compact and high amenity place to live, work and play. A 20-minute neighbourhood that strongly promotes walking and cycling to work, it will be a standout urban residential neighbourhood that delivers a high-quality urban design outcome. Designated as an Urban Design Performance Area (UDPA), development in this area is required to be assessed through performance-based criteria that set the benchmark for the subdivision, buildings, and road and active movement networks to ensure high quality outcomes.

Targeting higher densities than the surrounding neighbourhoods, approximately 1,600 homes will be delivered in the designated residential area alongside essential community infrastructure. The Officer South (Employment) community will be the life hub, bringing people, amenity, and community facilities to the precinct.

3.5.1 Place-based objectives – viable densities

OBJECTIVES

- | | |
|------------|---|
| O24 | To provide diversity of housing types and choice (lot size and physical form of housing) to meet community needs, including increased densities, accessible and specialised housing, and affordable and social housing options. |
| O25 | To deliver diverse streetscapes and neighbourhoods that support the type of density and housing to meet the quality of urban design outcomes. |
| O26 | To deliver a highly accessible, functional, and vibrant local town centre that encourages social interaction, focuses on activating the Lecky Road green spine with high quality architecture and active street frontages, and creates a strong sense of place. |
| O27 | To deliver an accessible network of neighbourhood community hubs across the precinct (particularly to local parks) that provide access to social, recreation, and health services that include flexible, adaptable design and facilitate a variety of uses. |
| O28 | To prepare for the impacts of climate change by facilitating resilient, environmentally sustainable design, subdivision and development. |

3.5.2 Place-based requirements and guidelines – viable densities

REQUIREMENTS

- | | |
|------------|---|
| R41 | Subdivisions must demonstrate a diversity of lot sizes to ensure that a range of dwelling types can be delivered in appropriate locations throughout the residential area. |
| R42 | Subdivisions and development must respond to Plan 3 Place Based Plan and Plan 10 Housing Plan with target densities and typologies per catchment as stated in Table 4 Housing density and diversity and Table 5 UDPA performance measures , to provide a diverse neighbourhood character with a range of lot sizes and dwelling types in appropriate locations throughout the residential area. |

GUIDELINES

- | | |
|------------|--|
| G40 | <p>Specialised housing forms, such as lifestyle communities, retirement living, aged care, affordable housing, labour hire accommodation or temporary crisis accommodation should:</p> <ul style="list-style-type: none"> • respond to and integrate with adjoining development, avoiding inactive interfaces and blank facades to the public street network • be located within a walkable catchment area shown on Plan 10 Housing Plan • be accessible by public transport and shared path networks • not present a barrier to movement through the surrounding road, public transport, pedestrian and active transport movement network • incorporate back-up power solutions to improve safety for vulnerable residents during power outages. |
|------------|--|

GUIDELINES

G41 Additional local convenience centre uses may be located in the designated mixed-use areas adjoining local parks subject to demonstrating that they do not compromise the role and function of the primary local town centre, to the satisfaction of the responsible authority. The ancillary centres are to provide opportunities for activating the local park, such as provision for cafes, small grocer, take-away and home office/small businesses without diminishing the density requirements for that area.

G42 An application for subdivision of land into residential lots or development of land for residential or mixed-use purposes should provide eight per cent minimum of all housing within the UDPA affordable and social housing, as defined by the *Planning and Environment Act 1987*.

G43 Subdivision should provide for a local street separating development from drainage reserves, sporting reserves and local parks, promoting an active interface. Where subdivision does not propose a local street,

- lots should be set back at least 4.5 metres and provide a 4 metre wide access way as the primary point of access along the lot frontage
- lots should allow for vehicular access via a rear laneway or shared driveway
- subdivision design should avoid side or rear fence treatments fronting open space and Drainage Services Scheme assets
- subdivision design should maximise opportunities for informal passive surveillance
- subdivision design should not limit the use of adjacent open space

all to the satisfaction of the responsible authority, and where adjacent to a waterway or drainage reserve to the satisfaction of the responsible authority and Melbourne Water.

G44 Screening of waste collection points, service and car parking areas should provide appropriate landscaping with planting of canopy trees to minimise amenity impacts with adjoining development, and heat island effect.

Table 4 Housing density and diversity

CATCHMENT	HOUSING TYPOLOGY	
	TARGET DENSITY	TARGET TYPOLOGIES
Amenity-based Density Area	Average 36 dwellings per ha	<p>To support diverse housing outcomes, subdivision applications must include opportunities for the delivery of multiple housing typologies. The mix of housing typologies may include, but is not limited to:</p> <ul style="list-style-type: none"> Integrated and apartment-style development Multi-unit development for sites near schools and community facilities Attached townhouse-style development Semi-detached/duplex-style development/<i>Small Lot Housing Code</i> product Low to mid-rise medium density development surrounding the school and amenity areas. <p>Mixed typology helps break up the density, create view corridors and provide a sensitive interface to the school.</p>
23 NDHA	Target quantity 823 dwellings	

CATCHMENT	HOUSING TYPOLOGY	
	TARGET DENSITY	TARGET TYPOLOGIES
Mixed Use Area 14 NDHA	Average 40 dwellings per ha Target quantity 570 dwellings	<p>To support diverse housing outcomes, subdivision applications must include opportunities for multiple housing typologies. The mix of housing typologies may include, but is not limited to:</p> <ul style="list-style-type: none"> • Attached townhouse-style development with small office accommodation • 2–3 Storey integrated and apartment style development • Walk-up low-rise apartment-style development • Shop-top retail low-rise (2–3 storey) residential development with cafés and local produce stores to activate parks. • Social and affordable, key worker accommodation near the local town centre and Lecky Road commercial precinct. <p>Encourage higher rise and density development to face and frame amenity and assist in creating a sense of place with passive surveillance and activity. Opportunity for key destination hubs at parks and schools promote opportunities for key worker accommodation located close to the bus capable road network.</p>
Town Centre Area 1 NDHA	Average 40 dwellings per ha Target quantity 40 dwellings	<p>To support diverse housing outcomes, subdivision applications must include opportunities for the delivery of multiple housing typologies within the centre to promote diversity. The mix of dwelling typologies may include, but is not limited to:</p> <ul style="list-style-type: none"> • 3–5 storey integrated and apartment style development with concealed integrated car parking • Walk-up low-rise apartment style development with concealed integrated car parking • Shop-top retail low-rise (2–3 storey) residential development for sites closer to the local parks • Social and affordable, key worker accommodation located close to the Lecky Rd commercial precinct. <p>High-density and high-quality development surrounding and within the local town centre to create destination points, aid with scale to create view corridors within the precinct and provide a sensitive interface to the community centre.</p>
Balance Area 6 NDHA	Average 32 dwellings per ha Target quantity 194 dwellings	<p>To support diverse housing outcomes, subdivision applications must include opportunities for the delivery of multiple housing typologies that promote affordable housing outcomes such as:</p> <ul style="list-style-type: none"> • Semi-detached/duplex-style development/Small Lot Housing Code product • Multi-unit affordable housing development for sites closer to the local parks • Detached traditional-style housing • Low-rise social housing projects • Attached townhouse development. <p>Low to mid-rise development to help diversify housing choice, create view corridors, and provide the opportunity for density relief.</p>

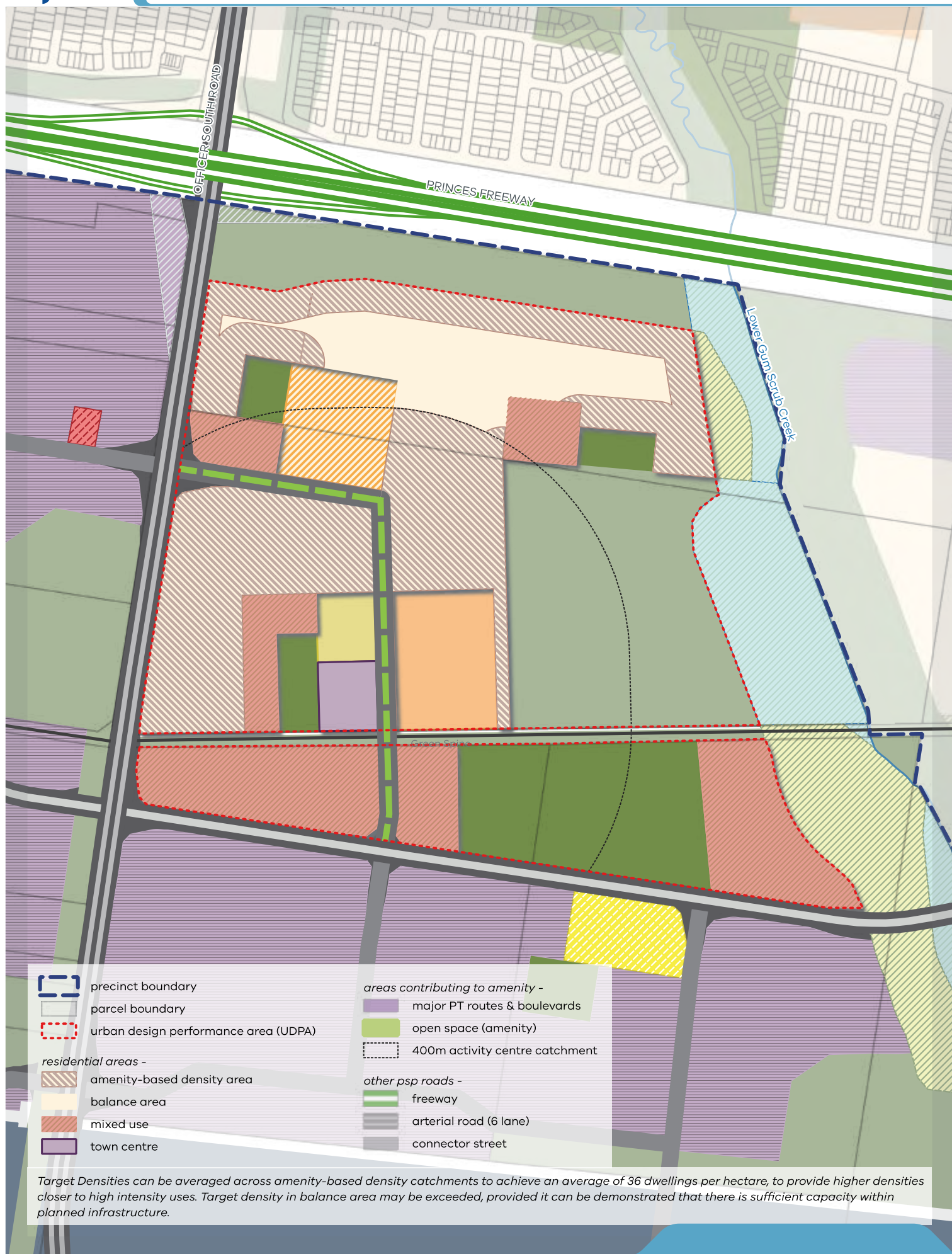
Table 5 UDPA performance measures

URBAN DESIGN PRIORITY ELEMENTS	PERFORMANCE REQUIREMENTS	
	REQUIRED PERFORMANCE CRITERIA	BENCHMARK OUTCOMES*
URBAN STRUCTURE	<ul style="list-style-type: none"> Provide a permeable and legible urban structure that enables a cohesive 20-minute neighbourhood and promotes identifiable precincts creating a clear understanding of place. Ensure a safe, highly pedestrianised public realm supported by good solar orientation and place-based features with equitable access to amenity and destinations. 	<ul style="list-style-type: none"> Varied block sizes are strategically placed to promote sub-precincts, diverse patterns and housing diversity that responds to the current and expected future markets. Streets positioned along a north-south or east-west axis promote rectangular lot formations. Clear view lines to the local town centre from key destinations are used to promote wayfinding and provide a connection with the Cardinia Road (Employment) Precinct community. Large lots or superlots are planned to accommodate future higher-density and/or mixed-use development. The green spine forms a linear park that is multifunctional, designed for active transport networks, intermittent exercise centres and meeting spaces to connect the community to the local town centre and activate surrounding areas. Linear parks are used to facilitate precincts of higher density or to promote vistas and are wide enough for practical use and council maintenance. The town square at the Local Town Centre integrates uses and provides a key destination for community functions. Small redundant areas within the UDPA are utilised as tree reserves in the public realm for lingering and shade, and to assist with the 30 per cent tree canopy target in R12.
	<ul style="list-style-type: none"> Ensure a varied and visually stimulating built environment through a range of building heights, scale, interface treatments (including to open space) and typologies. Provide a cohesive planned future character that promotes a sense of community, safety and inclusion by delivering housing diversity. 	<ul style="list-style-type: none"> Variations of built form and typology promote various sub-precinct characters. Rear-loaded typology is used to promote a safe pedestrian and canopy focus to reserves and mixed use areas. Buildings of over four levels are concentrated around the local town centre and commercial zoned areas to support a hub experience. Massing and building setbacks limit overshadowing of the public realm. Key sites at precinct gateways are earmarked for taller or distinctive built form outcomes to signify the important role they play in the centre as wayfinding features. Open public forecourts are located at strategic sites where the building interfaces the public realm at a height of four levels or more to improve visibility and solar access. Buildings are designed to promote passive surveillance.



URBAN DESIGN PRIORITY ELEMENTS	PERFORMANCE REQUIREMENTS	
	REQUIRED PERFORMANCE CRITERIA	BENCHMARK OUTCOMES*
MOVEMENT & SENSE OF PLACE	<p>Create a pedestrian-focused environment that:</p> <ul style="list-style-type: none"> connects people to services and destinations of natural and cultural amenity, including the wider context includes a safe and cohesive transport network provides places to linger encourages alternative modes of transport. 	<ul style="list-style-type: none"> A clear road hierarchy identifies direct routes to key destinations Pedestrian-centric zones are used at key locations The precinct is designed to support public transport including connections to the Officer Train Station via Officer South Road, and connections to Cardinia Road (Employment) Precinct and Minta Farm via Grices/Lecky Road. Key axis/corridors use cycle, and shared paths to create various journeys to designated meeting places. The public realm includes embellishments such as drinking fountains, seating and at ground lighting of paths to encourage all-hours use.

* A benchmark outcome is an example of how to achieve a performance. It is not a mandatory response. Other solutions may be presented for approval by the responsible authority.





3.6 Services and destinations

3.6.1 Place-based objectives – services and destinations

OBJECTIVES

- O29** To support education and community infrastructure and facilities that cater for multiple social needs, that are located to equitably and efficiently maximise their accessibility and shared use with adjoining precincts and the broader region.
- O30** To develop a local town centre that services the local and wider community with strong connections through Officer South Road and Lecky Road.
- O31** To deliver key destinations connected to the cycling and active path network that are designed within flexible open space to promote cultural and social activities and events.
- O32** To encourage investment and support education and training opportunities for technical industries within the local town centre where closely located with the commercial land uses.

3.6.2 Place-based requirements and guidelines – services and destinations

REQUIREMENTS

- R43** Proposed government school sites must have a minimum of two road frontages (three preferred), one of which must be a bus-capable connector road. All roads fronting school sites must be wide enough to simultaneously accommodate safe and efficient:
- pedestrian movement
 - two-way traffic and cycling movement
 - student drop-off zones, and indented parking of cars and buses.
- R44** Any lot created for a proposed government school site must be designed and serviced to the satisfaction of Department of Education.
- R45** Any lot created for a proposed state government facility must be designed and serviced to the satisfaction of Department of Health as identified in [Plan 3 Place Based Plan](#).

GUIDELINES

- G45** Subdivision and development should facilitate integration of schools, sports reserves, and community facilities and promote:
- integration with neighbouring facilities to maximise efficiencies through shared car parking and other complementary infrastructure
 - out-of-hours use, street activation and permeability
 - safe and convenient pedestrian and cyclist access.
- G46** Educational, community or civic infrastructure not shown on [Plan 3 Place Based Plan](#) should be located within or proximate to a local town centre and/or community facility, as appropriate.
- G47** Public health and justice services should be located within or adjacent to a community facility or local town centre and with access to public transport.
- G48** The land identified as a proposed state government facility may be used for an alternative interim purpose such as a local pop-up park, which is generally in accordance with the PSP and consistent with the provisions of the applied zone, until such time the Department of Health requires the land or the land is released for development in accordance with [G58](#). Any interim use must be consistent with the priorities of the Victorian Public Health and *Well-being Plan*. If and when the land is required by the Department of Health, the land must be finished to a standard that satisfies the requirements of the Department of Health prior to the transfer of the land back to the Department of Health.

GUIDELINES

- G49** Where the responsible authority is satisfied that land shown on [Plan 3 Place Based Plan](#) as proposed emergency services facility is unlikely to be used for this purpose, that land may be used for an alternative purpose that is compatible with the surrounding land uses and the provisions of the applied Commercial 2 zone.
- The development or subdivision of stage 1 of the PSP as shown in [Plan 12 Infrastructure and Development Staging](#) must be 80 per cent complete and the responsible authority must be in receipt of written advice from the Department of Health stating that the land is no longer required for the proposed emergency services facility.
-
- G50** Where the responsible authority is satisfied that land shown as a potential non-government school site is unlikely to be used for a non-government school, the land may be used for an alternative purpose which is generally in accordance with the PSP and consistent with the provisions of the applied zone. The development/subdivision of the residential area in stage 1 in the PSP as shown in [Plan 12 Infrastructure and Development Staging](#) must be 80 per cent complete and the responsible authority must be in receipt of a letter from the proposed education provider stating that the land is no longer required.
- The responsible authority must verify the need for the potential school with the education provider by referring to the *Background Report* and *Community Infrastructure Assessment* of the subject PSP area.
- Further guidance on this can be found in the VPA's '*Non-government School Planning Guidance Note*'.
-
- G51** The indicative configuration of the community facility, open space and sports field as illustrated in [Plan 3 Place Based Plan](#) and [Plan 6 Open Space and Community Facilities](#) may be altered to the satisfaction of the relevant responsible authorities. Where the alterations may impact a proposed government school site, the Department of Education should be consulted by the responsible authority to ensure that the changes are also to the satisfaction of that department.
-
- G52** Common boundaries of school sites with residential uses should be avoided where possible to avoid potential adverse amenity impacts on residential areas.
-
- G53** Drop off/pick up facilities for the proposed government primary school and adjacent kindergarten facility should be located in close proximity where practicable.
-
- G54** The amount of on street parking available on roads and streets which abuts a proposed government school site should be maximised.
-
- G55** Education, industry institutions and business training premises should be near key public and active transport links.
-
- G56** Ensure appropriate traffic management and road design is adopted around the development of the non-government primary school to limit congestion along the connector boulevard within the UDPA.
-
- G57** Land in the electricity transmission easement should be utilised to support community facilities related to walking, cycling and potential equestrian paths/trails (where feasible) as part of the broader open space network and service utilities, such as roads, renewable energy sources and the like. Use and development for purposes ancillary to adjacent industrial or employment uses (such as carparking areas) may also be appropriate.
-
- G58** Where the responsible authority is satisfied that land shown as a proposed state government facility is unlikely to be used for that purpose, the land may be used for an alternative purpose which is generally in accordance with the PSP and consistent with the provisions of the applied zone.
- The development or subdivision of stage 1 in the PSP as shown in [Plan 12 Infrastructure and Development Staging](#) (infrastructure and development staging) must be 80 per cent complete and the responsible authority must be in receipt of a letter from the Department of Health stating that the land is no longer required.
-





3.7 Infrastructure coordination

3.7.1 Place-based objectives – infrastructure coordination

OBJECTIVES

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|------------|---|
| O33 | To plan for development and infrastructure that encourages leveraging of existing and planned infrastructure, and the timely delivery of new infrastructure. |
| O34 | To actively pursue innovative and sustainable models for infrastructure delivery, and long-term strategic infrastructure opportunities that align with the UN Sustainable Development Goals. |
| O35 | To encourage environmentally sustainable design and development and encourage the use of sustainable energy across the precinct, including generation, storage and distribution. |
| O36 | To plan for an integrated water management system that reduces reliance on reticulated potable water, increases the re-use of alternative water through stormwater harvesting and water recycling contributing towards a sustainable and green urban environment. |
| O37 | To encourage diversion of waste from landfill, minimise waste generation and increase recovery of more resources with circular economy considerations. |
| O38 | Facilitate development that is adapted and resilient to climate-related hazards and supports the transition to net zero greenhouse gas emissions. |

3.7.2 Place-based requirements and guidelines – infrastructure coordination

REQUIREMENTS

- | | |
|------------|--|
| R46 | All above ground electricity cables (excluding substations and cables with a voltage of 66kv or greater) must be placed underground where feasible, including above ground cables as part of the upgrade of existing roads or subdivision works. |
| R47 | Above ground utilities (such as electricity substations, kiosk, and sewer pumps) must be identified at the subdivision design stage to ensure effective integration with the surrounding neighbourhood, to minimise amenity impacts and be designed to the satisfaction of the relevant authority. Where that infrastructure is intended to be in public open space, the land required to accommodate that infrastructure will not be counted as contributing to public open space requirements specified and will be additional to the areas designated in Table 2 Credited open space delivery . |
| R48 | The inclusion of public art and complementary infrastructure along pedestrian, bicycle and shared use paths and any potential equestrian trails must be consistent with the <i>Cardinia Shire Council Developer Landscape Guidelines 2017</i> (or as amended) and <i>Cardinia Shire Council Public Art Policy 2017</i> (or as amended). |
| R49 | Delivery of arterial roads and connector streets must prioritise construction to meet the adjacent property boundaries where a connection to the adjacent property is intended or indicated on Plan 12 Infrastructure and Development Staging , in accordance with the staging requirement of the PSP. For development abutting an arterial road, staging must prioritise the delivery of the road and drainage infrastructure, to the satisfaction of the responsible authority. |

REQUIREMENTS

R50 Development staging must be generally in accordance with [Plan 12 Infrastructure and Development Staging](#) and [Table 6 Precinct infrastructure](#) and must provide for the timely provision and delivery of the following infrastructure to the satisfaction of the responsible authority:

- connection to any arterial road network and seek to co-ordinate the delivery of these roads in conjunction with the timing of the arterial road connections located external to the precinct
- connector streets and connector street bridges
- street connections between properties, constructed to the property boundary
- on- and off-road pedestrian and bicycle network paths
- safe pedestrian path(s) (crushed rock or alternative interim provision where deemed appropriate) from any existing pedestrian network(s) to proposed connections to facilitate connectivity to services, transport, community infrastructure and adjoining communities
- drainage, flood protection, and stormwater quality treatment infrastructure to implement the applicable Development Services Scheme(s)
- essential infrastructure
- land for community infrastructure, sports fields, local open space (including urban agriculture).

R51 Staging of development must be determined by the development proposals on land within the precinct and the availability of infrastructure services, with reference to [Plan 12 Infrastructure and Development Staging](#). Development applications must demonstrate how the development will:

- integrate with adjoining developments, including the timely provision of roads and connections
- provide for the delivery of community facilities, open space, and amenity to new residents to the satisfaction of the responsible authority
- provide sealed road access to each new allotment
- provide safe pedestrian access to existing pedestrian networks
- deliver any necessary trunk service extensions, including confirmation of the agreed approach and timing by the relevant service provider to the satisfaction of the responsible authority
- deliver drainage, flood protection, and stormwater quality treatment infrastructure to implement the applicable Development Services Scheme(s)
- avoid and minimise impacts to conservation areas with regard to the location of essential and other services to the satisfaction of the responsible authority.

R52 Subdivision of land within the precinct must provide for and meet the cost for all local infrastructure, other than that provided for within the Officer South (Employment) ICP.

R53 All public open space (where not otherwise provided via the Officer South (Employment) ICP) must be finished to a standard that satisfies the requirements of the responsible authority prior to the transfer of the public open space.

R54 Where an inter-parcel connection is intended or indicated in the PSP, streets must be constructed to property boundaries at the relevant stage of development required or approved by the responsible authority. Provision should be made for temporary vehicle turning until the inter-parcel connection is delivered.

R55 Other than perpendicular road crossings of the high-pressure gas easement, no road or carriageway easements are to be relocated on gas pipeline easements unless to the satisfaction of the pipeline owner and operator.

R56 Development must ensure sensitive land uses are minimised within the consequence area shown on [Plan 13 Utilities](#) in [Appendix 1 Utilities](#), subject to planning controls responding to the high-pressure gas easement and that construction is managed to minimise risk of any adverse impacts.

Clear access must be provided to the APA City Gate Facility at all times, to the satisfaction of the operator and facility owner.



REQUIREMENTS

- R57** Utilities must be placed outside of conservation areas and waterway corridors identified in [Plan 3 Place Based Plan](#). Where services cannot avoid crossing or being located within a conservation area or waterway corridor, they must consolidate utilities into dedicated service corridors where possible, and be located to avoid disturbance to existing waterway values, native vegetation, significant landform features, heritage sites and habitats, to the satisfaction of DEECA, Melbourne Water, the appointed RAP and the responsible authority.
- R58** Development must provide for the delivery of waterway and drainage infrastructure, including stormwater detention, quality treatment, and volume control (as applicable), and consider opportunities for early establishment of waterways, to the satisfaction of Melbourne Water and the responsible authority.
- Where it can be demonstrated to the satisfaction of Melbourne Water that delivery of drainage infrastructure in its ultimate form is not possible, development proposals must demonstrate how any interim solution adequately manages flow rates and flow volume, treats stormwater generated from the development (without causing adverse impacts to other properties within or outside the precinct, the environment, cultural heritage, or other infrastructure) and how this will enable delivery of an ultimate drainage solution, to the satisfaction of Melbourne Water and the responsible authority.
- Development construction staging and interim solutions must avoid or mitigate the risk of soil erosion and water and waterway degradation from sodic and/or dispersive soils.
- R59** Where there are no alternative options other than passing through BCS conservation areas, subdivision and development must consolidate utilities into dedicated service corridors.
- R60** Any lot created for a proposed sewer pump station site must be designed and serviced to the satisfaction of South East Water.

GUIDELINES

- G59** Development should aim for net zero carbon emissions and seek to provide opportunities for local renewable energy generation technology and infrastructure.
- G60** The delivery of underground services should be coordinated, located, and bundled (using common trenching) to maintain the cross-section widths of paths and nature strips as shown in the PSP and to facilitate trees and other planting within the road reserve.
- G61** Subdivision design should provide emergency services with good access/egress to the arterial road network to maximise coverage and reduce response times.
- G62** All new above-ground utilities, including temporary utilities, should be located outside of key view lines, and screened, to the satisfaction of the responsible authority.
- G63** The design and location of underground services and trunk services should be guided by requirements as per [Appendix 8 Design and location of underground services](#) and be placed along general alignments as advised by the relevant servicing authorities.
- G64** Utilities should generally be located within the road reserve. Where this is not practical, or disrupts the ability to create 30 per cent tree canopy coverage in accordance with [R12](#), easements to place utilities at the rear of lots may be considered where there is no practical alternative.
- G65** Staging of development in the Gum Scrub Creek catchment should prioritise the delivery of the Gum Scrub Priority Works shown on [Plan 9 Integrated Water Management](#).
- G66** Infrastructure projects identified in [Plan 11 Precinct Infrastructure Plan](#) should be delivered as per the timing priority identified in the timing column of [Table 6 Precinct infrastructure](#).
- Note:** Project delivery timing outlined in [Table 6](#) is indicative and subject to periodic review by the relevant responsible authority.

GUIDELINES

G67 Any utility infrastructure running adjacent to or crossing a high-pressure gas pipeline should cross at 90 degrees unless approved otherwise by the responsible authority and be engineered to protect the integrity of the asset to the satisfaction of the responsible authority and gas pipeline owner/operator.

G68 The existing gas distribution assets will require a 2.1 metre clearance from title boundaries and a 3.0m clearance from proposed building facades, inclusive of verandas and canopies.

G69 Development or subdivision may locate the high-pressure gas easement within private lot boundaries where lot frontage contains car parking and landscaping, subject to the satisfaction of the asset manager. In this scenario, the asset manager retains legal access to the pipeline.

Approval from the asset manager and responsible authority must be obtained prior to proceeding with any construction.

G70 For all landscaping, paths and public furniture/exercise equipment proposed within a gas easement, a landscape plan must be submitted to the responsible authority in consultation with the service owner/operator demonstrating species, their location and who will be responsible for the ongoing management of landscaping within the easement.

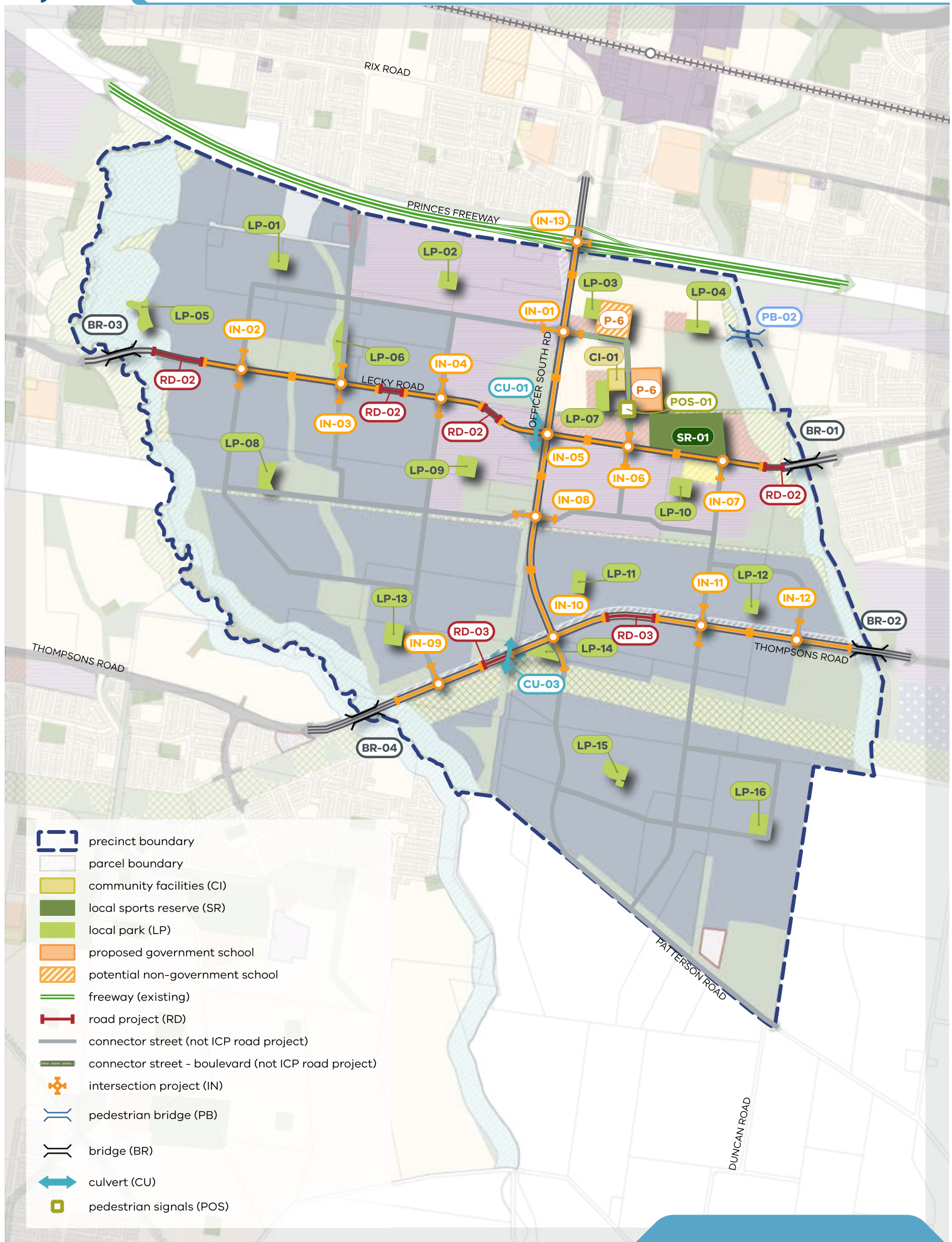
G71 Sports fields, community facilities, local parks and playgrounds should be delivered as early as possible within the residential neighbourhood and may be delivered in stages.

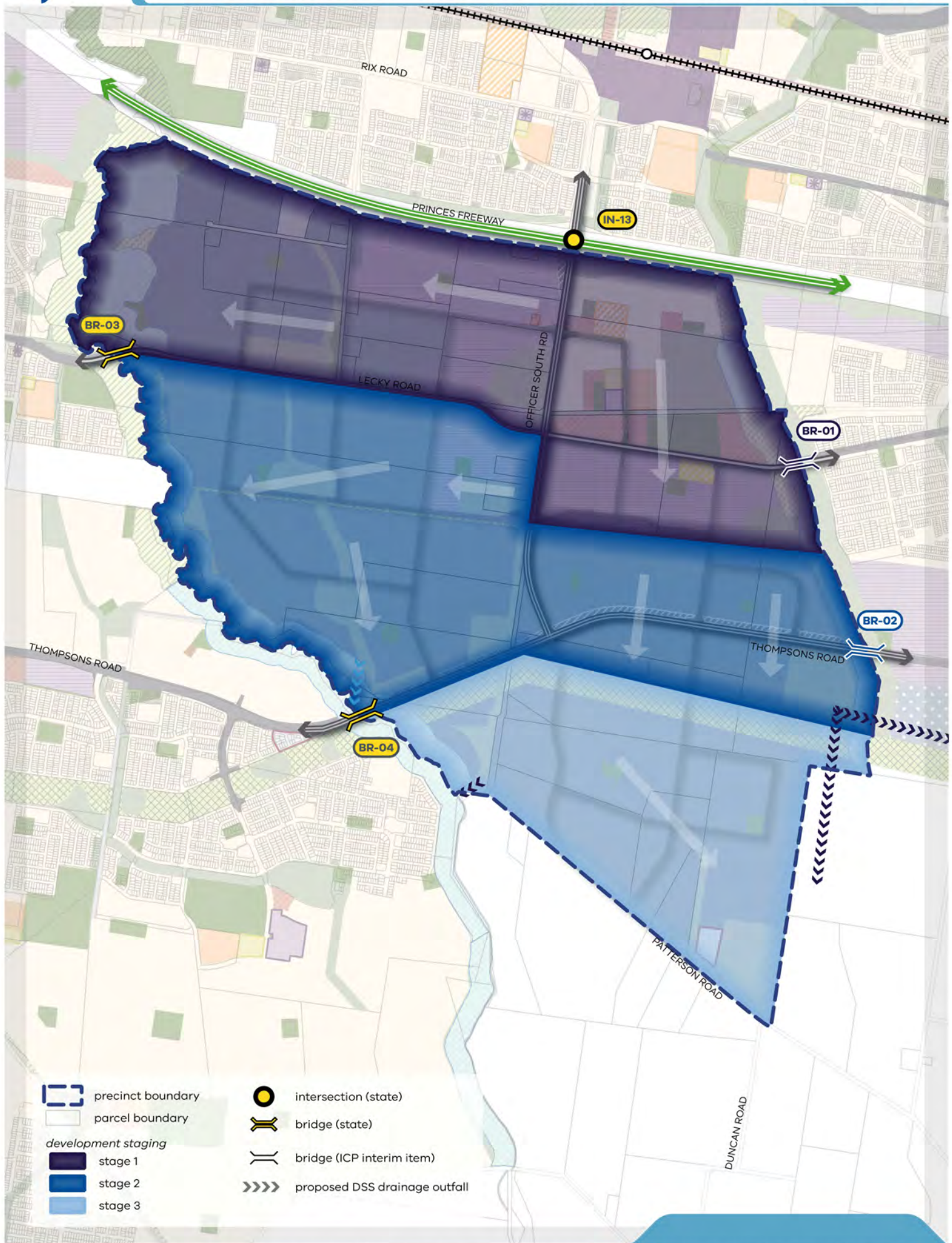
G72 Development should consider Environmentally Sustainable Development principles, such as the inclusion of, but not limited to:

- material re-use and recycling (use of materials with reduced embodied energy)
- electrical self-generation, car charge schemes, smart grids, and battery storage
- use of tools such as Built Environment Sustainability Scorecard (BESS), Sustainable Subdivisions Framework & Green Star Buildings
- measures that reduce the urban heat island effect
- waste management initiatives
- development that should facilitate the reduction of environmental impacts and resource use through:
 - public realm design and connectivity
 - facilitation of alternative energy generation systems
 - access to public and integrated active transport networks.

G73 Road design, line markings, traffic signs and other road infrastructure should, where practical, meet best practice standards for autonomous vehicles including shuttle services.

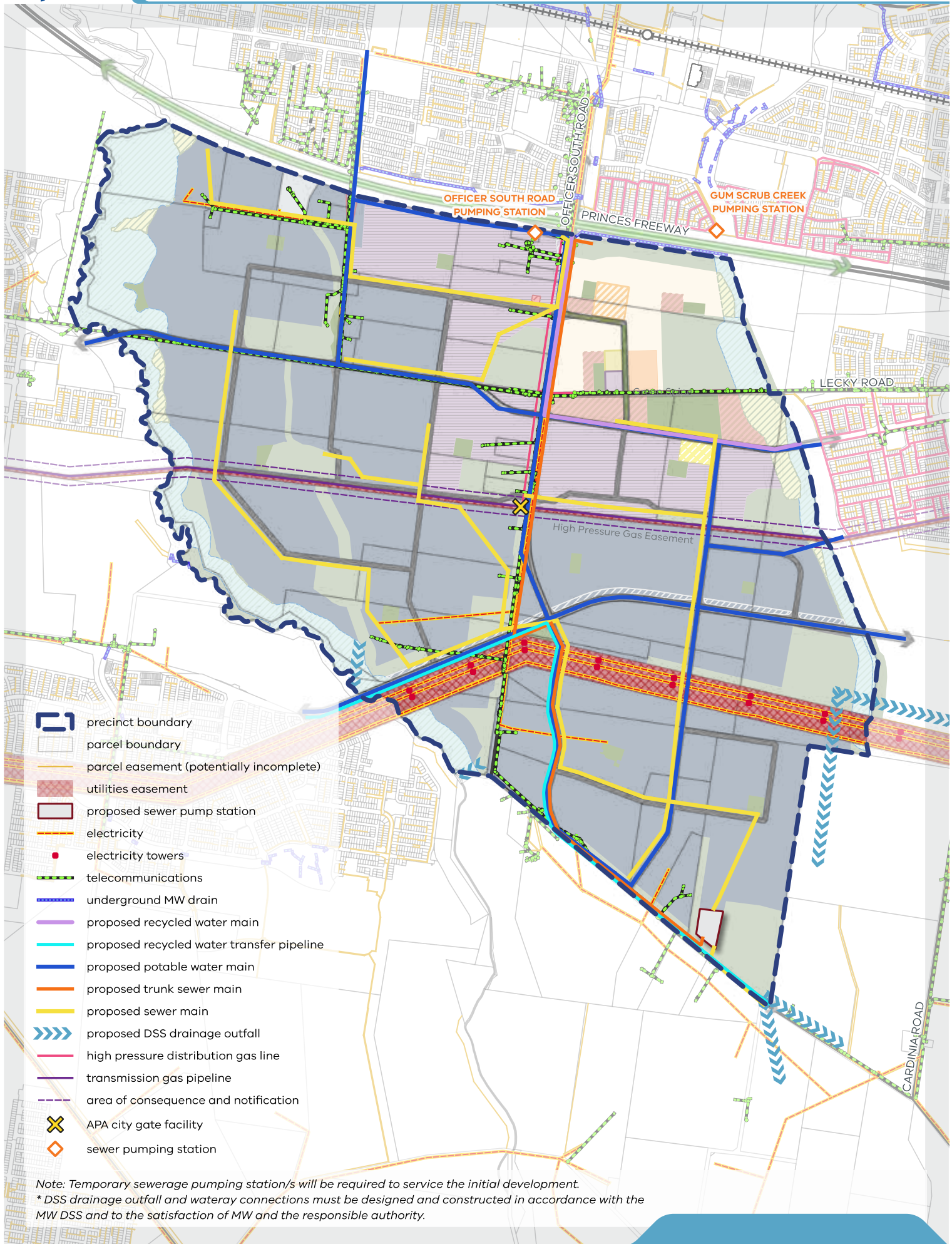




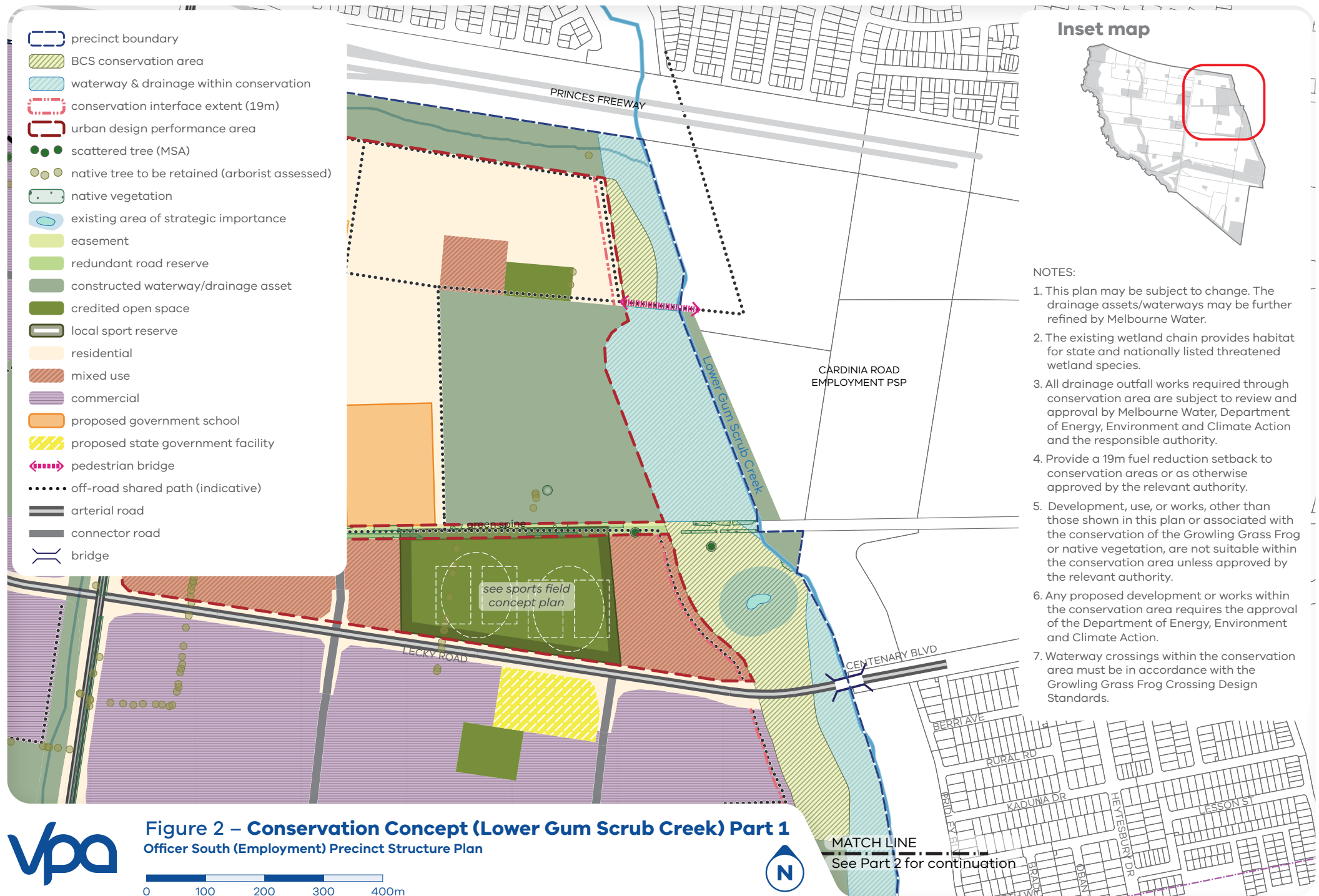


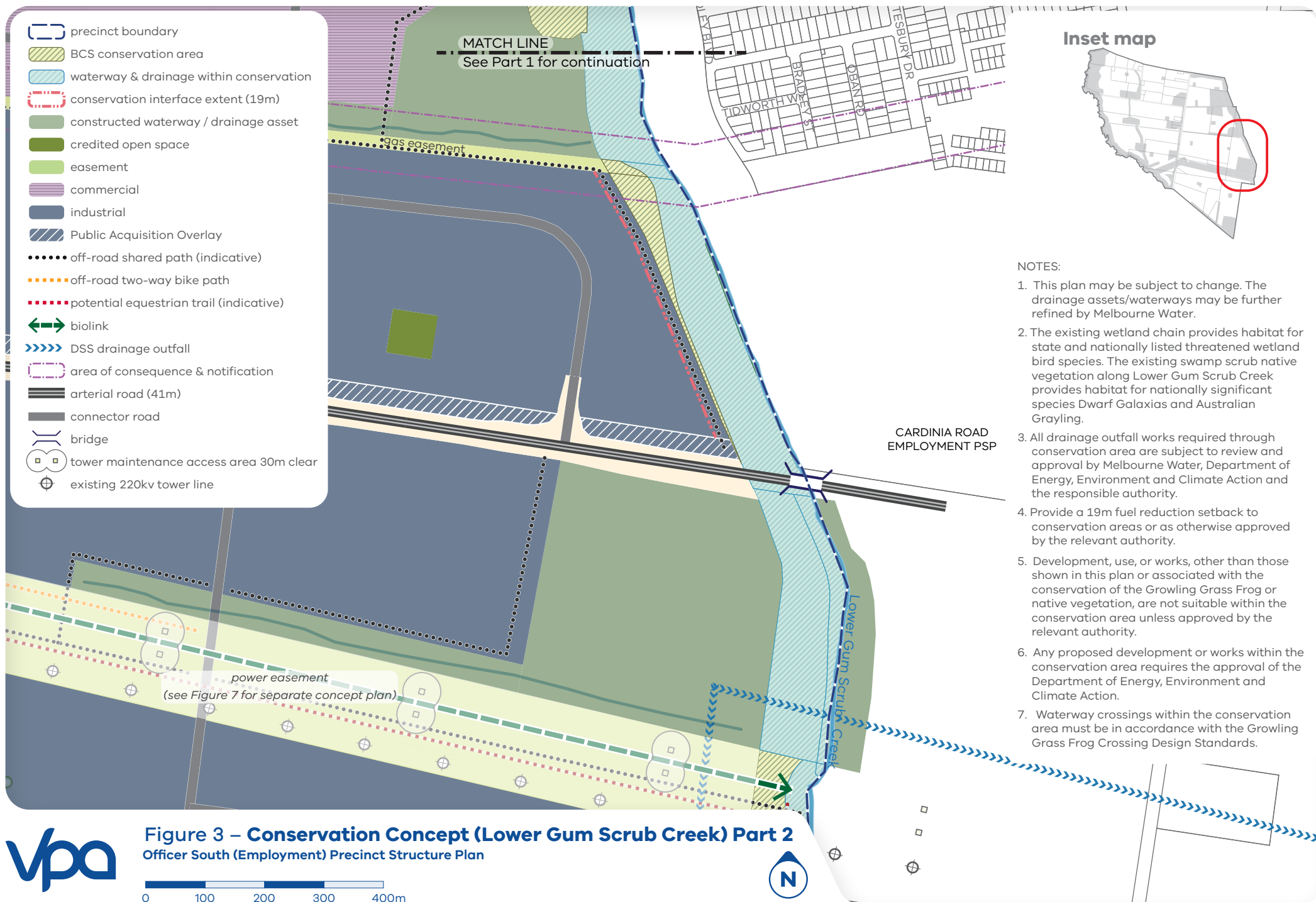
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Appendix 2 Conservation area concept plans





Inset map



NOTES:

1. This plan may be subject to change. The drainage assets/waterways may be further refined by Melbourne Water.
2. The existing wetland chain provides habitat for state and nationally listed threatened wetland bird species. The existing swamp scrub native vegetation along Cardinia Creek provides habitat for nationally significant species Dwarf Galaxias and Australian Grayling.
3. All drainage outfall works required through conservation area are subject to review and approval by Melbourne Water, Department of Energy, Environment and Climate Action and the responsible authority.
4. Provide a 19m fuel reduction setback to conservation areas or as otherwise approved by the relevant authority.
5. Potential equestrian trail underpass at Grices and Thompsons Road is subject to the final Drainage Service Scheme and clearance.
6. Development, use, or works, other than those shown in this plan or associated with the conservation of the Growling Grass Frog or native vegetation, are not suitable within the conservation area unless approved by the relevant authority.
7. Any proposed development or works within the conservation area requires the approval of the Department of Energy, Environment and Climate Action.
8. Waterway crossings within the conservation area must be in accordance with the Growling Grass Frog Crossing Design Standards.

- precinct boundary
- conservation interface extent (19m)
- cultural value area
- scattered tree (MSA)
- native tree to be retained (arborist assessed)
- native vegetation
- native vegetation – swamp scrub
- existing area of strategic importance
- credited open space
- BCS conservation area
- waterway & drainage within conservation
- constructed waterway / drainage asset
- industrial
- Victorian Heritage Inventory site
- shared path – off-road (indicative)
- potential equestrian trail (indicative)
- arterial road
- connector street (industrial indicative)
- bridge

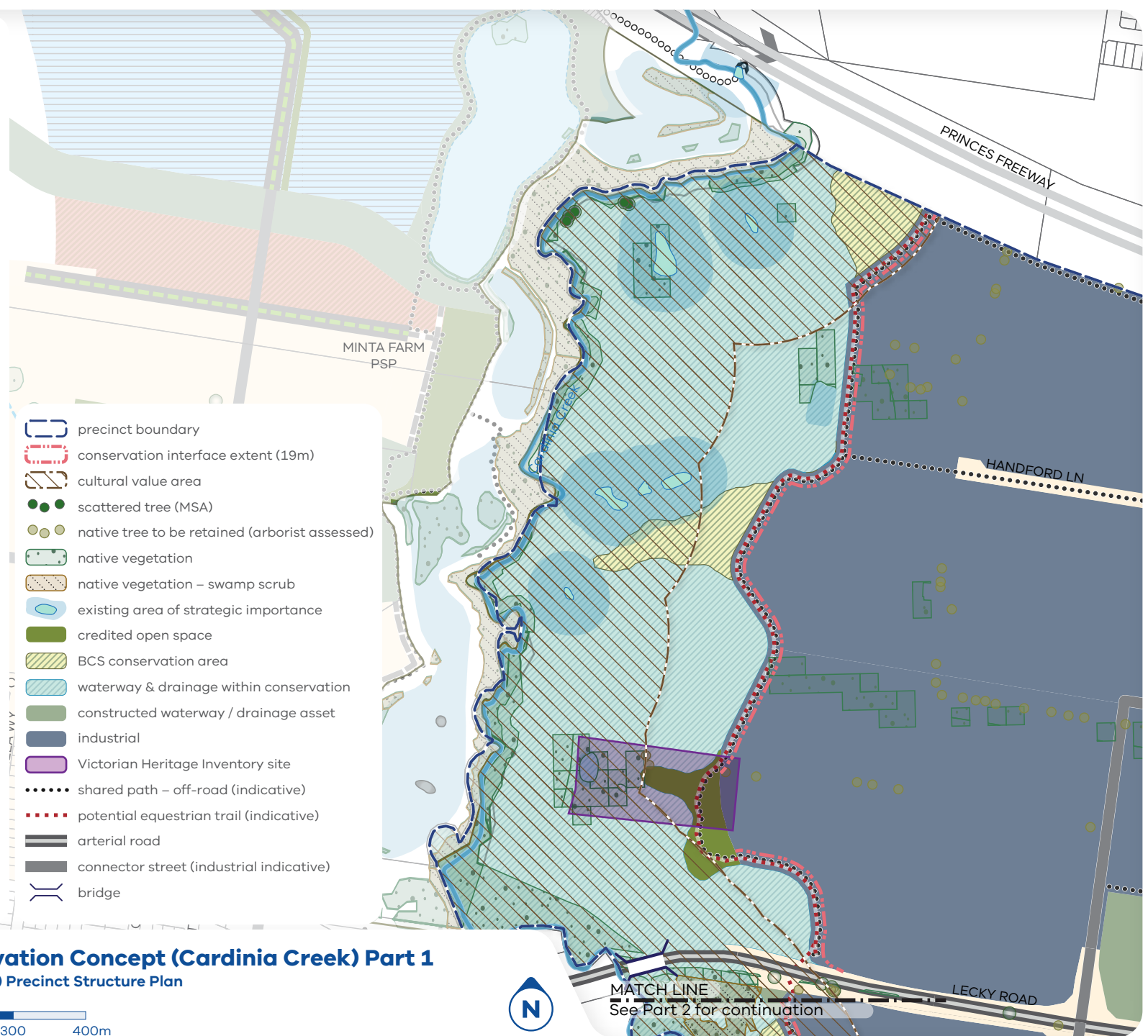
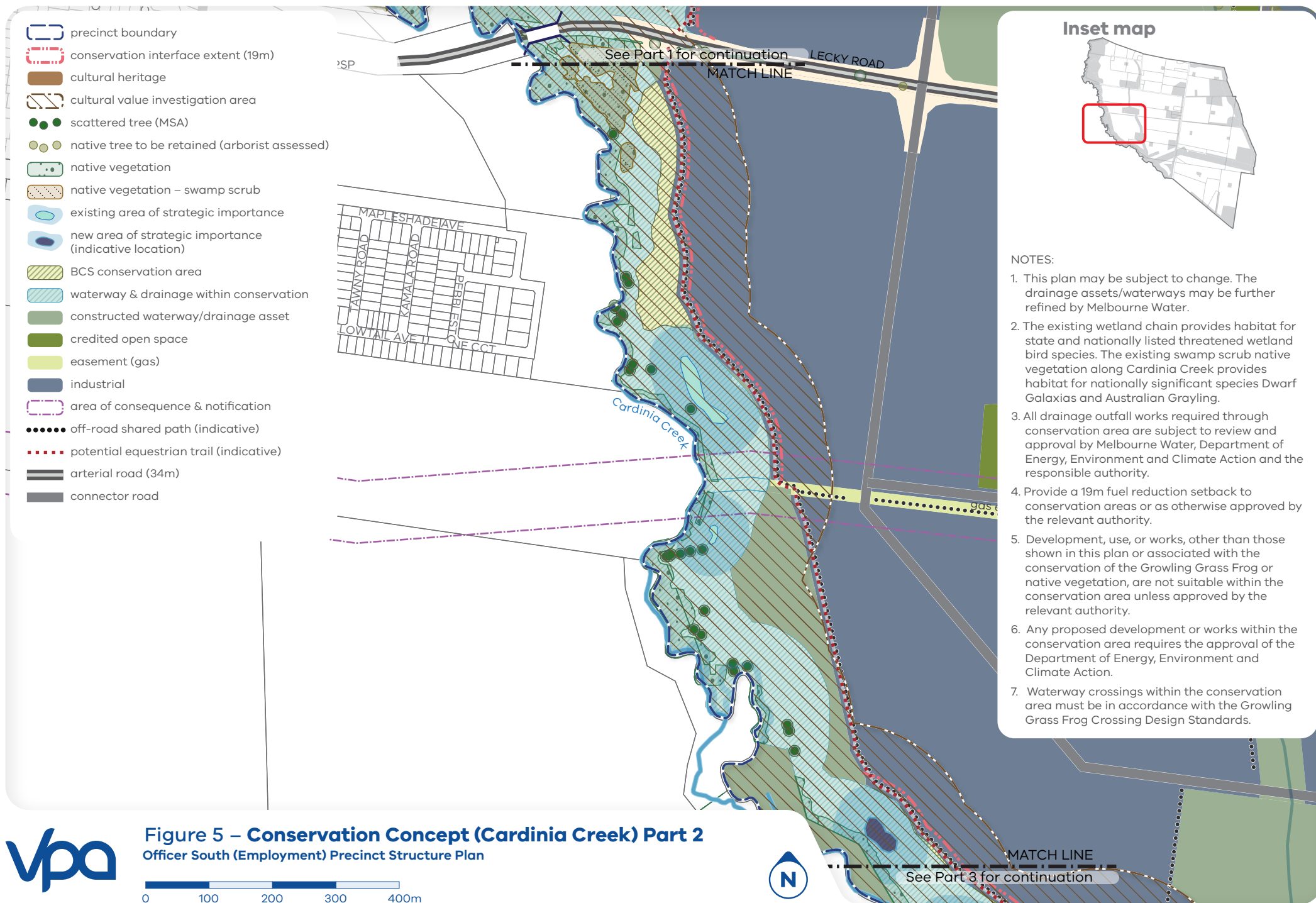


Figure 4 – Conservation Concept (Cardinia Creek) Part 1
Officer South (Employment) Precinct Structure Plan



MATCH LINE
See Part 2 for continuation



Inset map



NOTES:

1. This plan may be subject to change. The drainage assets/waterways may be further refined by Melbourne Water.
2. The existing wetland chain provides habitat for state and nationally listed threatened wetland bird species. The existing swamp scrub native vegetation along Cardinia Creek provides habitat for nationally significant species Dwarf Galaxias and Australian Grayling.
3. All drainage outfall works required through conservation area are subject to review and approval by Melbourne Water, Department of Energy, Environment and Climate Action and the responsible authority.
4. Provide a 19m fuel reduction setback to conservation areas or as otherwise approved by the relevant authority.
5. Development, use, or works, other than those shown in this plan or associated with the conservation of the Growling Grass Frog or native vegetation, are not suitable within the conservation area unless approved by the relevant authority.
6. Any proposed development or works within the conservation area requires the approval of the Department of Energy, Environment and Climate Action.
7. Waterway crossings within the conservation area must be in accordance with the Growling Grass Frog Crossing Design Standards.

- precinct boundary
- proposed local heritage overlay
- cultural value area
- Public Acquisition Overlay
- conservation interface extent (19m)
- scattered tree (MSA)
- native tree to be retained (arborist assessed)
- potential high value tree (council assessed)
- native vegetation
- new area of strategic importance (indicative location)
- BCS conservation area
- waterway & drainage within conservation
- constructed waterway/drainage asset
- credited open space
- easement (electricity)
- industrial

- off-road shared path (indicative)
- potential equestrian trail (indicative)
- biolink
- DSS drainage outfall
- arterial road
- connector road
- bridge
- tower maintenance access area 30m clear
- existing 220kv tower line

See Part 2 for continuation
MATCH LINE

Cardinia Creek

Growling Grass Frog
conservation area outside
PSP boundary

THOMPSONS ROAD

power easement
(see Figure 7 for
separate concept plan)

BANJO PLACE



Figure 6 – Conservation Concept (Cardinia Creek) Part 3
Officer South (Employment) Precinct Structure Plan

0 100 200 300 400m



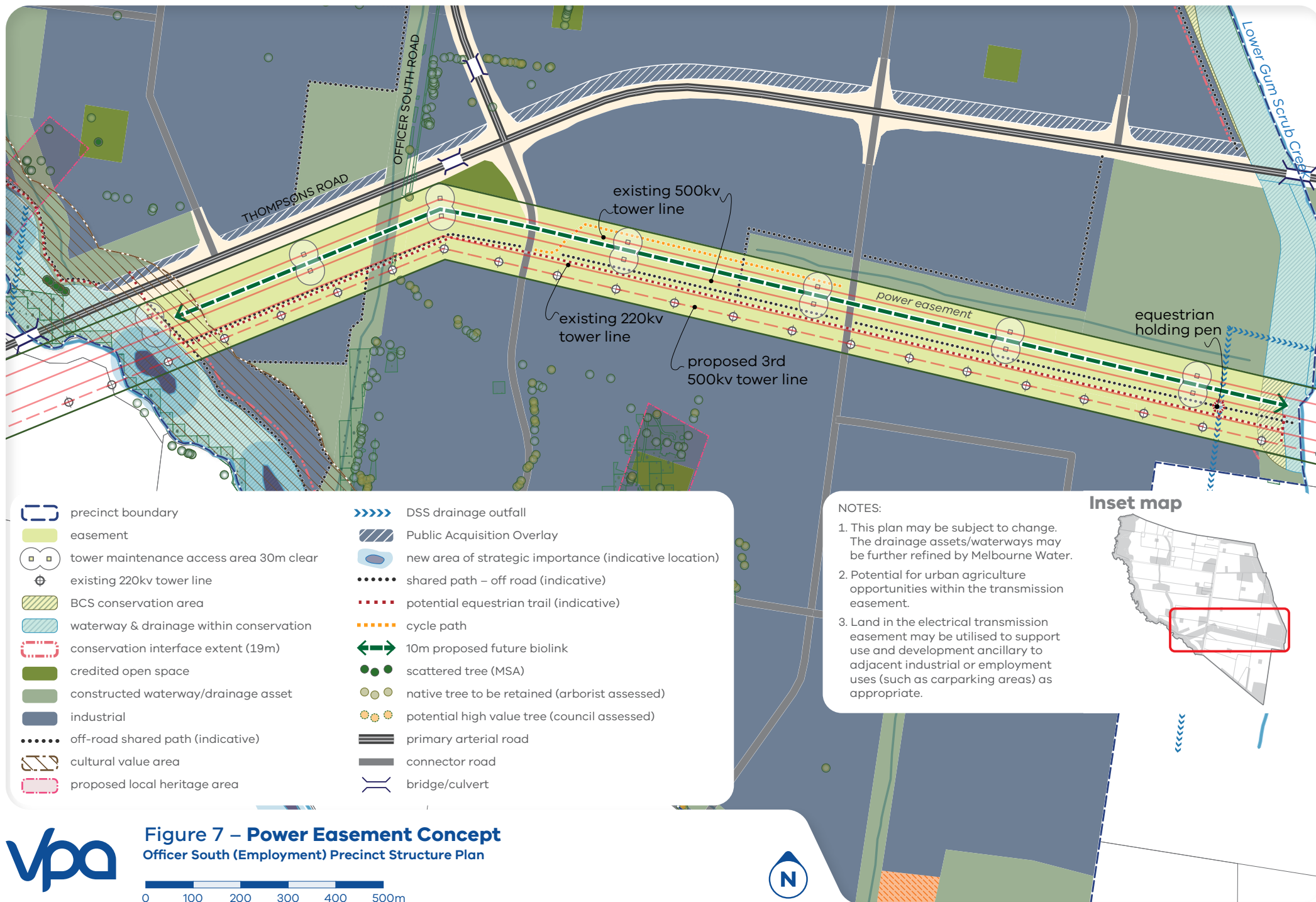
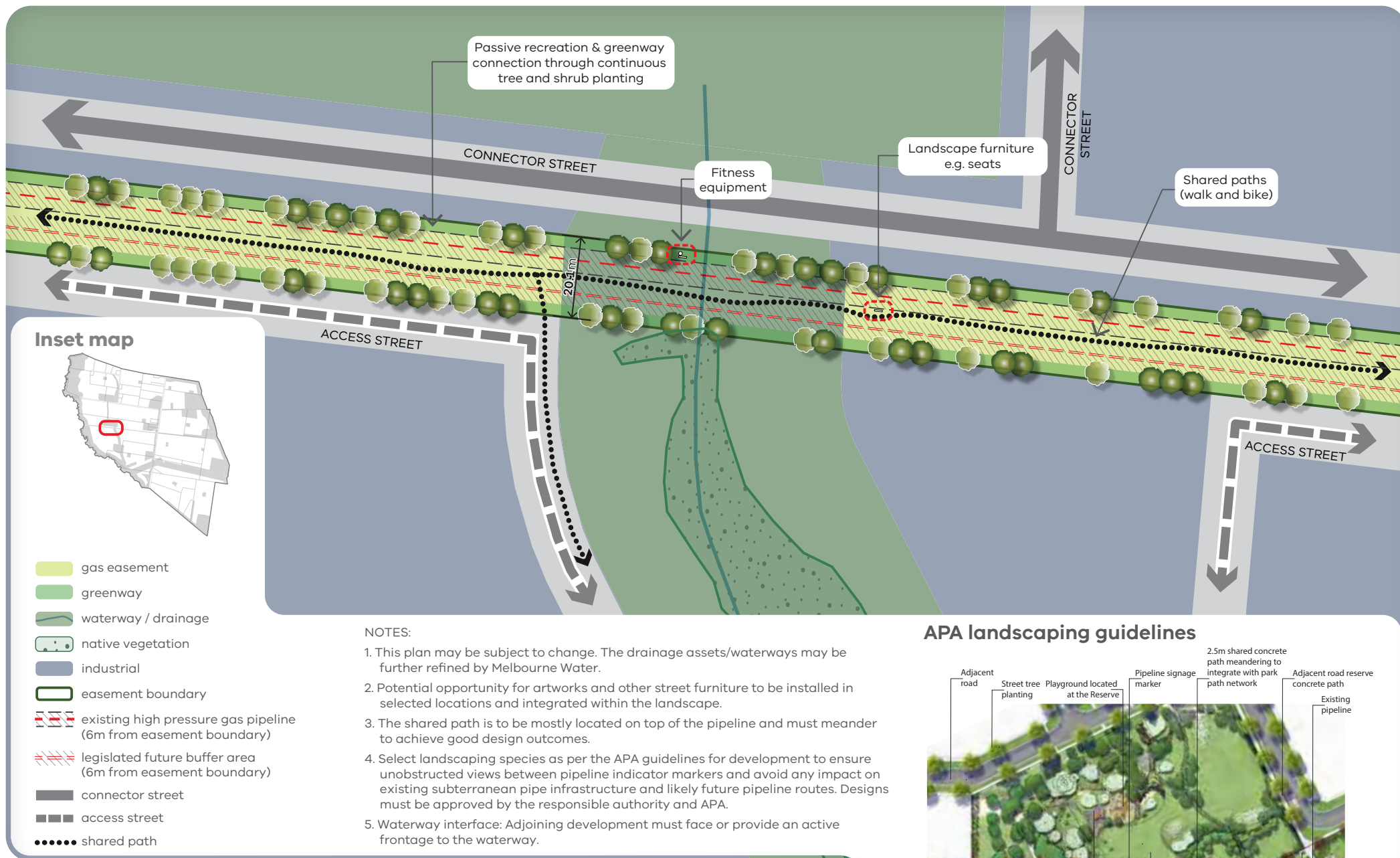


Figure 7 – Power Easement Concept
Officer South (Employment) Precinct Structure Plan



Appendix 4 Precinct infrastructure

Table 6 Precinct infrastructure

PIP REFERENCE NO.	TITLE & DESCRIPTION	LEAD AGENCY	COMPONENT INCLUDED IN ICP			TIMING	APPORTIONMENT FUNDING SOURCE	APPORTIONMENT TO THE ICP
			ULTIMATE LAND	INTERIM CONSTRUCTION	ULTIMATE CONSTRUCTION			
TRANSPORT								
OS-PB-02	Cardinia Road (Employment) Precinct Pedestrian Bridge Construction of a shared pedestrian and cyclist bridge over Lower Gum Scrub Creek into Cardinia Road (Employment) Precinct	Cardinia Shire Council	No	No	Yes	M	Cardinia Shire	50.00%
OS-BR-01	Lecky Road Bridge Construction of a secondary arterial road bridge (interim standard) over Lower Gum Scrub Creek into the Cardinia Road (Employment) Precinct PSP in accordance with the Growling Grass Frog Conservation Area requirements	Cardinia Shire Council	No	Yes	No	S–M	Cardinia Road (Employment) Precinct/ Cardinia Shire	50.00%
OS-BR-02	Thompsons Road Bridge Construction of an arterial road bridge (interim standard) over Lower Gum Scrub Creek into the Cardinia Road (Employment) Precinct PSP in accordance with the Growling Grass Frog Conservation Area requirements	Cardinia Shire Council	No	Yes	No	M–L	Cardinia Road (Employment) Precinct/ Cardinia Shire	50.00%
OS-BR-03	Grices Road Bridge Construction of a secondary arterial road bridge (ultimate) over Cardinia Creek into Clyde North PSP in accordance with the Growling Grass Frog Conservation Area requirements	Department of Transport	No	No	No	M–L	N/A	–
OS-BR-04	Thompsons Road Bridge Construction of an arterial road bridge (ultimate) over Cardinia Creek into the Cardinia Creek South PSP in accordance with the Growling Grass Frog Conservation Area requirements	Department of Transport	No	No	No	L	N/A	–
OS-IN-01	Intersection: Officer South Road/east–west Connector (north) Construction of an interim primary arterial to connector signalised 4-way intersection	Cardinia Shire Council	Yes	Yes	No	S–M	N/A	100.00%

PIP REFERENCE NO.	TITLE & DESCRIPTION	LEAD AGENCY	COMPONENT INCLUDED IN ICP			TIMING	APPORTIONMENT FUNDING SOURCE	APPORTIONMENT TO THE ICP
			ULTIMATE LAND	INTERIM CONSTRUCTION	ULTIMATE CONSTRUCTION			
OS-IN-02	Intersection: Lecky Road/north–south Connector (west) Construction of an interim secondary arterial to connector signalised 4-way intersection	Cardinia Shire Council	Yes	Yes	No	M	N/A	100.00%
OS-IN-03	Intersection: Lecky Road/north–south Connector (west) Construction of an interim secondary arterial to connector signalised 4-way intersection	Cardinia Shire Council	Yes	Yes	No	M	N/A	100.00%
OS-IN-04	Intersection: Lecky Road/north–south Connector (west) Construction of an interim secondary arterial to connector signalised 4-way intersection	Cardinia Shire Council	Yes	Yes	No	S–M	N/A	100.00%
OS-IN-05	Intersection: Lecky Road/Officer South Road Construction of a primary arterial to secondary arterial 4-way signalised intersection	Cardinia Shire Council	Yes	Yes	No	S–M	N/A	100.00%
OS-IN-06	Intersection: Lecky Road/north–south Connector (east) Construction of a secondary arterial to connector signalised 4-way intersection	Cardinia Shire Council	Yes	Yes	No	S–M	N/A	100.00%
OS-IN-07	Intersection: Lecky Road/north–south Connector (east) Construction of an interim secondary arterial to connector signalised T-intersection	Cardinia Shire Council	Yes	Yes	No	S–M	N/A	100.00%
OS-IN-08	Intersection: Officer South Road/east–west Connector (central) Construction of a primary arterial to connector signalised 4-way intersection	Cardinia Shire Council	Yes	Yes	No	S–M	N/A	100.00%
OS-IN-09	Intersection: Thompsons Road/north Connector (west) Construction of a primary arterial to connector signalised T-intersection	Cardinia Shire Council	Yes *	Yes	No	L	N/A	100.00%
OS-IN-10	Intersection: Officer South Road/Thompsons Road Construction of a primary arterial to primary arterial – 4-way signalised intersection	Cardinia Shire Council	Yes *	Yes	No	M–L	N/A	100.00%
OS-IN-11 (Standard Levy)	Intersection: Thompsons Road/north–south Connector (east) Construction of a primary arterial to connector – 4-way signalised intersection	Cardinia Shire Council	Yes *	Yes	No	M–L	N/A	91.27%

PIP REFERENCE NO.	TITLE & DESCRIPTION	LEAD AGENCY	COMPONENT INCLUDED IN ICP			TIMING	APPORTIONMENT FUNDING SOURCE	APPORTIONMENT TO THE ICP
			ULTIMATE LAND	INTERIM CONSTRUCTION	ULTIMATE CONSTRUCTION			
OS-IN-11 (Supplementary Levy)	Intersection: Thompsons Road/north-south Connector (east) Construction of a primary arterial to connector –4-way signalised intersection.	Cardinia Shire Council	Yes *	Yes	No	M–L	N/A	8.73%
OS-IN-12	Intersection: Thompsons Road/ north-south Connector (east) Construction of a primary arterial to connector signalised–T-intersection.	Cardinia Shire Council	Yes *	Yes	No	L	N/A	100.00%
OS-IN-13	Intersection: Officer South Freeway Interchange Ultimate signalisation and provision of primary arterial under freeway	Department of Transport	No	No	No	M	N/A	–
OS-CU-01	Lecky Road Culvert Construction of culvert crossing over the 40m wide drainage corridor of secondary arterial interim	Cardinia Shire Council	N/A	No	Yes	S–M	N/A	100.00%
OS-CU-03	Thompsons Road Culvert (depending on alignment of Thompsons Road) Construction of culvert crossing over the 50m wide drainage corridor of primary arterial interim	Cardinia Shire Council	N/A	No	Yes	M	N/A	100.00%
OS-RD-02	Lecky Road: Arterial Road Construction of 2 lanes in one carriageway in a road reservation with a width of 34 metres.	Cardinia Shire Council	Yes	Yes	No	S–M	N/A	100.00%
OS-RD-03	Thompsons Road: Arterial Road Construction of 2 lanes in one carriageway in a road reservation with a width of 41 metres.	Cardinia Shire Council	Yes	Yes	No	M–L	N/A	100.00%
OS-POS-01	Connector Boulevard Pedestrian Operated Signals Construction of pedestrian operated signals along the connector boulevard	Cardinia Shire Council	N/A	N/A	Yes	S–M	N/A	100.00%
OS-PP-01	Plan preparation costs Plan preparation costs	Victorian Planning Authority	N/A	N/A	N/A	S	Third Party Funding	47.47%

PIP REFERENCE NO.	TITLE & DESCRIPTION	LEAD AGENCY	COMPONENT INCLUDED IN ICP			TIMING	APPORTIONMENT FUNDING SOURCE	APPORTIONMENT TO THE ICP
			ULTIMATE LAND	INTERIM CONSTRUCTION	ULTIMATE CONSTRUCTION			
COMMUNITY & EDUCATION								
OS-CI-01	Level 2 Community Facility Construction of a Level 2 multipurpose community centre (including 4× kindergartens, community rooms, neighbourhood house, consulting suites for outreach visiting services and additional facilities to cater for childcare and 2× maternal child health)	Cardinia Shire Council	Yes	N/A	Yes	S–M	N/A	45.00%
N/A	State Government Health Facility Purchase of land and construction of a proposed State Government Health Facility	Department of Health	No	No	No	S–M	N/A	–
N/A	Green spine Proposed P6 Government School Land and construction of government school (incorporating potential joint-use oval with adjoining council sports reserve as relevant)	Department of Education	No	No	No	S–M	N/A	–
N/A	Non-government School Land and construction of non-government school.	Catholic Diocese	No	No	No	S–M	N/A	–
UTILITIES								
N/A	Drainage Infrastructure Land and construction of precinct drainage infrastructure	Melbourne Water	N/A	N/A	N/A	M–L	N/A	–
N/A	Sewer Pump Station Purchase of land and construction of a proposed Sewer Pump Station	South East Water	No	No	No	M	N/A	–
OPEN SPACE								
OS-SR-01	Sports fields Construction of a local sports reserve (comprising 2× Australian Rules Football/cricket ovals, 2× netball courts, 2× soccer fields)	Cardinia Shire Council	Yes	No	Yes	S–M	N/A	45.00%
OS-SR-01c	Sports fields Construction of multi-purpose pavilion	Cardinia Shire Council	Yes	No	Yes	S–M	N/A	45.00%
OS-LP-01	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	S–M	N/A	100.00%
OS-LP-02	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	S–M	N/A	100.00%

PIP REFERENCE NO.	TITLE & DESCRIPTION	LEAD AGENCY	COMPONENT INCLUDED IN ICP			TIMING	APPORTIONMENT FUNDING SOURCE	APPORTIONMENT TO THE ICP
			ULTIMATE LAND	INTERIM CONSTRUCTION	ULTIMATE CONSTRUCTION			
OS-LP-03	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	S-M	N/A	100.00%
OS-LP-04	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	S-M	N/A	100.00%
OS-LP-05	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	S-M	N/A	100.00%
OS-LP-06	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	S-M	N/A	100.00%
OS-LP-07	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	S-M	N/A	100.00%
OS-LP-08	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	M-L	N/A	100.00%
OS-LP-09	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	M-L	N/A	100.00%
OS-LP-10	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	S-M	N/A	100.00%
OS-LP-11	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	M-L	N/A	100.00%
OS-LP-12	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	M-L	N/A	100.00%
OS-LP-13	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	L	N/A	100.00%
OS-LP-14	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	L	N/A	100.00%
OS-LP-15	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	L	N/A	100.00%
OS-LP-16	Passive Open Space Provision of land for a local park	Cardinia Shire Council	Yes	No	No	L	N/A	100.00%
N/A	Cardinia Creek Conservation Areas Service Open Space: Land and provision of conservation areas	DEECA	N/A	N/A	N/A	M-L	N/A	-

PIP REFERENCE NO.	TITLE & DESCRIPTION	LEAD AGENCY	COMPONENT INCLUDED IN ICP			TIMING	APPORTIONMENT FUNDING SOURCE	APPORTIONMENT TO THE ICP
			ULTIMATE LAND	INTERIM CONSTRUCTION	ULTIMATE CONSTRUCTION			
N/A	Lower Gum Scrub Creek Conservation Areas Service Open Space: Land and provision of conservation areas	DEECA	N/A	N/A	N/A	M-L	N/A	–

* Denotes the ultimate land take for OS-IN-09, OS-IN-10, OS-IN-11 and OS-IN-12 relates to the 41 metre wide road reservation for OS-RD-03.

Note: Project ID OS-RD-01 (Officer South Road) is not included as Officer South Road is composed of back-to-back intersections.

-  precinct boundary
-  parcel boundary (PSP reference number)
-  arterial road - existing road reserve
-  arterial road - public acquisition overlay
-  arterial road - new/widening/intersection flaring
-  non-arterial road - retained existing road reserve
-  proposed government school
-  potential non-government school
-  local community facility
-  proposed emergency services facility
-  proposed state government facility
-  BCS conservation reserve
-  waterway & drainage within conservation
-  waterway & drainage reserve
-  utilities easement
-  redundant road reserve
-  local sports reserve
-  local park
-  local park (employment areas)
-  utility sub-station / facility
-  existing developed land
-  NDA - residential
-  NDA - employment

Table 7 Summary land use budget

DESCRIPTION	HECTARES	% OF TOTAL	% OF NDA
TOTAL PRECINCT AREA	1,068.76		
TRANSPORT			
Arterial road – existing road reserve	8.26	0.77%	1.19%
Arterial road – Public Acquisition Overlay	6.62	0.62%	0.95%
Arterial road – new/widening/intersection flaring (ICP land)	29.97	2.80%	4.32%
Non-arterial road – new/widening/intersection flaring (ICP land)	-	-	-
Non-arterial road – retained existing road reserve	4.87	0.46%	0.70%
SUB-TOTAL TRANSPORT	49.72	4.65%	7.17%
COMMUNITY & EDUCATION			
ICP community facilities	1.00	0.09%	0.14%
Proposed government school	3.50	0.33%	0.50%
Potential non-government school	3.00	0.28%	0.43%
Potential government facility/emergency services	2.25	0.21%	0.32%
SUB-TOTAL EDUCATION	9.75	0.91%	1.41%
OPEN SPACE			
UNCREDITED OPEN SPACE & REGIONAL OPEN SPACE			
BCS conservation reserve	14.12	1.32%	2.04%
Cultural value investigation area	-	-	-
Waterway and drainage reserve	134.14	12.55%	19.35%
Waterway and drainage within conservation	95.99	8.98%	13.85%
Utilities easements	42.78	4.00%	6.17%
Redundant road reserve	1.66	0.16%	0.24%
SUB-TOTAL UNCREDITED OPEN SPACE & REGIONAL OPEN SPACE	288.69	27.01%	41.65%
CREDITED OPEN SPACE			
Local network park (ICP land)	13.88	1.30%	2.00%
Local sports reserve (ICP land)*	7.82	0.73%	1.13%
SUB-TOTAL CREDITED OPEN SPACE	21.71	2.03%	3.13%
TOTAL ALL OPEN SPACE	310.16	29.02%	44.73%
OTHER			
Utilities sub-stations/facilities (acquired by relevant authority)	2.00	0.19%	0.29%
Existing developed land	3.74	0.35%	0.54%
SUB-TOTAL OTHER	5.74	0.54%	0.83%
TOTAL NET DEVELOPABLE AREA (NDA) Ha	693.16	64.86%	
TOTAL NET DEVELOPABLE AREA – RESIDENTIAL (NDA-R) Ha	43.82	4.10%	
TOTAL NET DEVELOPABLE AREA – EMPLOYMENT (NDA-E) Ha	649.34	60.76%	

* The total sports reserve area is 8.5 hectares, including 0.68 hectares of green spine land.

Note: Minor discrepancies in numbers due to rounding.

Table 8 Parcel-specific land use budget

PARCEL ID	TOTAL AREA (HA)	ARTERIAL ROAD – EXISTING ROAD RESERVE	ARTERIAL ROAD – PUBLIC ACQUISITION OVERLAY	ARTERIAL ROAD – NEW/ WIDENING/INTERSECTION FLARING (ICP LAND)	NON-ARTERIAL ROAD – RETAINED EXISTING ROAD RESERVE	GOVERNMENT SCHOOL	POTENTIAL NON- GOVERNMENT SCHOOL	ICP COMMUNITY FACILITIES	BCS CONSERVATION RESERVE	WATERWAY & DRAINAGE RESERVE	UTILITIES EASEMENTS	LOCAL SPORTS RESERVE (ICP LAND)	LOCAL NETWORK PARK (ICP LAND)	UTILITIES SUB-STATIONS/ FACILITIES (ACQUIRED BY RELEVANT AUTHORITY)	POTENTIAL GOVERNMENT FACILITY/EMERGENCY SERVICES	REDUNDANT ROAD RESERVE	WATERWAY & DRAINAGE WITHIN CONSERVATION	EXISTING DEVELOPED LAND	TOTAL NET DEVELOPABLE AREA (HA)	TOTAL CONTRIBUTION LAND (HA)	TRANSPORT (HA)	RESIDENTIAL COMMUNITY AND RECREATION (HECTARES)	COMMERCIAL & INDUSTRIAL COMMUNITY AND RECREATION (HA)
Total PSP	1,068.76	8.26	6.62	29.97	4.87	3.50	3.00	1.00	14.12	134.14	42.78	7.82	13.88	2.00	2.25	1.66	95.99	3.74	693.16	745.83	29.97	11.32	11.38
OS-01	2.54	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	2.54	–	–	–	–	–	–
OS-02	20.85	–	–	–	–	–	–	–	1.03	–	–	–	–	–	–	–	12.07	–	7.75	7.75	–	–	–
OS-03	8.32	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	8.32	8.32	–	–	–
OS-04	3.74	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	3.74	–	–	–	–	–
OS-05	3.01	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	3.01	3.01	–	–	–
OS-06	1.00	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	1.00	1.00	–	–	–
OS-07	21.52	–	–	–	–	–	–	–	–	–	–	–	0.77	–	–	–	–	–	20.75	21.52	–	–	0.77
OS-08	11.23	–	0.63	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	10.59	10.59	–	–	–
OS-09	1.38	–	0.17	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	1.21	1.21	–	–	–
OS-10	2.00	–	0.04	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	1.96	1.96	–	–	–
OS-11	29.66	–	0.27	0.29	–	–	1.66	–	1.25	7.01	–	–	1.50	–	–	–	2.10	–	15.58	17.37	0.29	1.50	–
OS-12	54.79	–	–	0.99	–	–	–	–	1.39	2.92	–	–	1.00	–	–	–	18.17	–	30.32	32.32	0.99	–	1.00
OS-13	0.81	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	0.81	0.81	–	–	–
OS-14	12.00	–	–	–	–	–	–	–	–	–	–	–	0.47	–	–	–	–	–	11.53	12.00	–	–	0.47
OS-15	12.00	–	–	–	–	–	–	–	–	1.86	–	–	0.30	–	–	–	–	–	9.84	10.14	–	–	0.30
OS-16	12.00	–	–	0.25	–	–	–	–	–	6.08	–	–	0.98	–	–	–	–	–	4.69	5.92	0.25	–	0.98
OS-17	0.77	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	0.77	0.77	–	–	–
OS-18	12.07	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	12.07	12.07	–	–	–
OS-19	1.00	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	1.00	1.00	–	–	–
OS-20	1.00	–	–	0.16	–	–	–	–	–	–	–	–	–	–	–	–	–	–	0.84	1.00	0.16	–	–
OS-21	10.84	–	–	0.31	–	–	–	–	–	–	–	–	–	–	–	–	–	–	10.53	10.84	0.31	–	–
OS-22	10.00	–	–	0.36	–	–	–	–	–	–	–	–	–	–	0.25	–	–	–	9.39	9.75	0.36	–	–
OS-23	12.14	–	–	0.16	–	–	–	–	–	2.70	–	–	–	–	–	–	–	–	9.27	9.44	0.16	–	–

PARCEL ID	TOTAL AREA (HA)	ARTERIAL ROAD – EXISTING ROAD RESERVE	ARTERIAL ROAD – PUBLIC ACQUISITION OVERLAY	ARTERIAL ROAD – NEW/ WIDENING/INTERSECTION FLARING (ICP LAND)	NON-ARTERIAL ROAD – RETAINED EXISTING ROAD RESERVE	GOVERNMENT SCHOOL	POTENTIAL NON-GOVERNMENT SCHOOL	ICP COMMUNITY FACILITIES	BCS CONSERVATION RESERVE	WATERWAY & DRAINAGE RESERVE	UTILITIES EASEMENTS	LOCAL SPORTS RESERVE (ICP LAND)	LOCAL NETWORK PARK (ICP LAND)	UTILITIES SUB-STATIONS/ FACILITIES (ACQUIRED BY RELEVANT AUTHORITY)	POTENTIAL GOVERNMENT FACILITY/EMERGENCY SERVICES	REDUNDANT ROAD RESERVE	WATERWAY & DRAINAGE WITHIN CONSERVATION	EXISTING DEVELOPED LAND	TOTAL NET DEVELOPABLE AREA (HA)	TOTAL CONTRIBUTION LAND (HA)	TRANSPORT (HA)	RESIDENTIAL COMMUNITY AND RECREATION (HECTARES)	COMMERCIAL & INDUSTRIAL COMMUNITY AND RECREATION (HA)
OS-24	12.12	–	–	0.43	–	–	–	–	–	6.38	–	–	–	–	–	–	–	–	5.31	5.74	0.43	–	–
OS-25	45.51	–	–	1.22	–	3.50	1.34	1.00	–	13.32	–	–	1.00	–	–	–	6.04	–	18.10	21.31	1.22	2.00	–
OS-26	4.46	–	–	0.06	–	–	–	–	–	–	–	–	–	–	–	–	4.40	–	–	0.06	0.06	–	–
OS-28	78.81	0.00	–	3.54	–	–	–	–	1.35	2.74	–	–	1.00	–	–	–	3.45	–	66.74	71.27	3.54	–	1.00
OS-29	2.00	–	–	0.07	–	–	–	–	–	0.36	–	–	–	–	–	–	–	–	1.57	1.63	0.07	–	–
OS-30E	30.89	–	–	2.51	–	–	–	–	–	–	–	–	–	–	–	–	–	–	28.38	30.89	2.51	–	–
OS-30R	10.15	–	–	1.55	–	–	–	–	–	–	–	2.36	–	–	–	–	–	–	6.24	10.15	1.55	2.36	–
OS-31E	25.35	–	–	1.32	–	–	–	–	0.15	4.63	–	–	1.00	–	2.00	–	–	–	16.24	18.56	1.32	–	1.00
OS-31R	14.78	–	–	0.94	–	–	–	–	4.13	0.33	–	5.46	–	–	–	–	0.01	–	3.90	10.31	0.94	5.46	–
OS-32	127.44	–	–	0.25	–	–	–	–	0.41	20.64	3.36	–	1.00	–	–	–	11.31	–	90.47	91.72	0.25	–	1.00
OS-33	2.00	–	–	0.07	–	–	–	–	–	1.93	–	–	–	–	–	–	–	–	–	0.07	0.07	–	–
OS-34	2.00	–	–	0.19	–	–	–	–	–	1.81	–	–	–	–	–	–	–	–	–	0.19	0.19	–	–
OS-35	24.99	–	–	–	–	–	–	–	–	4.26	–	–	–	–	–	–	–	–	20.74	20.74	–	–	–
OS-36	76.64	–	2.96	7.45	–	–	–	–	–	–	2.46	–	1.20	–	–	–	–	–	62.57	71.23	7.45	–	1.20
OS-37	77.71	–	1.22	2.88	–	–	–	–	4.07	19.35	9.84	–	–	–	–	–	14.08	–	26.27	29.15	2.88	–	–
OS-38	8.57	–	–	–	–	–	–	–	–	2.54	–	–	–	–	–	–	6.03	–	–	–	–	–	–
OS-39	39.99	–	1.14	3.31	–	–	–	–	–	8.61	5.80	–	1.00	–	–	–	4.52	–	15.61	19.92	3.31	–	1.00
OS-40	6.89	–	0.20	1.32	–	–	–	–	–	–	–	–	0.47	–	–	–	–	–	4.90	6.69	1.32	–	0.47
OS-41	44.07	–	–	0.34	–	–	–	–	–	2.29	18.91	–	0.20	–	–	–	–	–	22.33	22.86	0.34	–	0.20
OS-42	1.69	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	–	1.69	1.69	–	–	–
OS-43	18.31	–	–	–	–	–	–	–	–	3.70	1.39	–	–	–	–	–	8.56	–	4.66	4.66	–	–	–
OS-44	35.14	–	–	–	–	–	–	–	–	–	–	–	1.00	–	–	–	–	–	34.14	35.14	–	–	1.00
OS-45	25.99	–	–	–	–	–	–	–	–	1.70	–	–	–	–	–	–	–	–	24.29	24.29	–	–	–
OS-46	17.64	–	–	–	–	–	–	–	–	0.72	–	–	–	–	–	–	–	–	16.93	16.93	–	–	–
OS-47	7.35	–	–	–	–	–	–	–	–	1.09	–	–	–	–	–	–	–	–	6.25	6.25	–	–	–
OS-48	60.71	–	–	–	–	–	–	–	–	16.48	–	–	1.00	2.00	–	–	–	–	41.24	42.24	–	–	1.00

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OS-49	0.28	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.28	-	-	-	-	-	-
OS-50	0.19	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.19	-	-	-	-	-	-
OS-51	0.24	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.24	-	-	-	-	-	-
OS-52	0.01	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.01	-	-	-	-	-	-
OS-53	0.11	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.11	-	-	-	-	-	-
OS-54	0.27	-	-	-	-	-	-	-	-	0.07	-	-	-	-	-	-	0.20	-	-	-	-	-	-
OS-55	0.24	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.24	-	-	-	-	-	-
OS-56	0.27	-	-	-	-	-	-	-	-	0.05	-	-	-	-	-	-	0.22	-	-	-	-	-	-
OS-57	0.21	-	-	-	-	-	-	-	0.02	0.19	-	-	-	-	-	-	-	-	-	-	-	-	-
OS-58	0.40	-	-	-	-	-	-	-	0.01	0.12	-	-	-	-	-	-	0.27	-	-	-	-	-	-
OS-59	0.33	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.33	-	-	-	-	-	-
OS-60	0.02	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.02	-	-	-	-	-	-
Sub-total	1,048.42	0.00	6.62	29.97	-	3.50	3.00	1.00	13.82	133.88	41.76	7.82	13.88	2.00	2.25	-	95.40	3.74	689.79	742.46	29.97	11.32	11.38
OS-R1	7.60	4.45	-	-	-	-	-	-	-	0.02	0.41	-	-	-	-	-	-	-	2.73	2.73	-	-	-
OS-R2	5.05	3.77	-	-	-	-	-	-	-	0.19	-	-	-	-	-	-	0.44	-	0.65	0.65	-	-	-
OS-R3	2.67	0.04	-	-	-	-	-	-	0.30	-	0.62	-	-	-	-	1.66	0.05	-	-	-	-	-	-
OS-R4	1.82	-	-	-	1.82	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
OS-R5	1.55	-	-	-	1.55	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
OS-R6	1.65	-	-	-	1.50	-	-	-	-	0.05	-	-	-	-	-	-	0.10	-	-	-	-	-	-
Sub-total	20.34	8.26	-	-	4.87	-	-	-	0.30	0.26	1.02	-	-	-	-	1.66	0.59	-	3.37	3.37	-	-	-

Appendix 6 Local town centre design criteria

A6.1 Local town centre performance requirements and example benchmark outcomes

Local Town Centre Principle 1

Create a sense of place with high quality engaging urban design and built form

Relevant key elements

- Urban structure
- Built form and interfaces
- Services
- Urban Character
- Noise

Performance criteria

- The centre must address all relevant elements of the Urban Design Guidelines of Victoria
- Development blocks must be based on a permeable layout with varying block sizes to enable flexibility to suit a variety of land uses and allow viable short-term development as well as efficient long-term evolution
- The local town centre core must integrate with the surrounding neighbourhoods, public transport, and community facilities, and transition the building scale to its edges
- The centre must seek to minimise amenity and noise impacts resulting from the mix of uses
- The design of each building must contribute to a cohesive and legible character for the local town centre as a whole
- The built form must define the primary street frontage with visually rich, interesting, and well-articulated facades, finished in suitable materials and colours that contribute to the character of the local town centre
- Car parking and service areas must be well integrated and screened
- Mixed-use areas should primarily provide retail and/or office and commercial at ground level and residential above ground level.

Example benchmark outcome

- Retail uses along street frontages include access points at regular intervals to encourage activity along the length of the street
- Retail and commercial buildings within the centre are generally built to the property line
- The village square and green spine act as a forecourt to the mixed use and centre, and act as a focal point for surrounding retail and residential uses
- Urban art and street furniture is incorporated into the public realm in areas that are highly visible and close to or adjoining pedestrian desire lines/gathering spaces
- Specialty retail and mixed use sleeve the anchor retail core and village square
- Car parking edges are wrapped with built form to improve the street interface
- Service areas present a well-designed and secure facade to public
- Car parking areas are screened from the main street with appropriate landscaping, planted with canopy trees and provided with dedicated pedestrian thoroughfares
- Key strategic buildings are located in the centre along view lines, including (as appropriate) clear glazing and regular entrances, an appropriate range of building material/colour palette themes and architectural design treatments (including opportunities for signage integration into building design)
- Separation areas such as open space, road networks and transitional scale is provided to address community facilities and outdoor function spaces
- Development on the west side of the boulevard connector provides for ground floor specialist suites (office, medical services, etc) and independent retail and hospitality tenancies
- Grouping waste collection points are planned for to maximise opportunities for recycling and reuse, and screened to minimise amenity impacts with adjoining areas and users of the centre.

Local Town Centre Principle 2

Design the local town centre to be safe, pedestrian-friendly and accessible by all modes of transport, while enabling private vehicle access.

Relevant key elements:

- Transportation modes
- Passive surveillance

Performance criteria

- The centre must provide a safe and accessible transport and active transport network, including bike parking, bike stations and vehicular charging stations
- The centre must provide active frontages that address connector streets and boulevard connectors as a main street frontage
- Movement must be prioritised for pedestrians, then cycling, then lastly vehicular within the core of the centre
- Public transport must be readily accessible from the centre and boulevard connector street
- Services must be provided to promote lingering in the public realm.

Example benchmark outcome

- The green spine promotes the active transport network to integrate the local town centre core with the surrounding neighbourhoods, public transport, and community facilities
- Separate pedestrian and cyclist paths are delivered on the green spine, to integrate the local town centre core with the surrounding neighbourhoods, public transport, and community facilities
- Bicycle parking is provided within the street network and public spaces in highly visible locations and close to pedestrian desire lines and key destinations
- Car parking areas should be located centrally to the site and to the rear and or side of street-based retail frontages. Car parking areas should be designated to ensure passive surveillance and public safety through adequate positioning and lighting
- Car parking areas should be designed to provide dedicated pedestrian routes and areas of landscaping
- On-street car parking should be provided either as parallel or angle parking to encourage short stay parking
- Car parking ingress and egress crossovers should be grouped and limited. Car parking ingress or egress and car parking areas accommodating heavy vehicle movements should be designed to limit the pedestrian/vehicle conflict
- Streets, public spaces, and car parks should be well lit to Australian standards and with pedestrian-friendly (generally white) light. Lighting should be designed to avoid unnecessary spill to the side or above
- Smart City connectivity is provided including wi-fi in the village square
- Building frontages incorporate the use of a consistent covered walkway or veranda to provide for weather protection
- The main streets are designed to include canopy tree provision, outdoor dining and pedestrian activity with minimal on-street parking

Local Town Centre Principle 3

Promote localisation, sustainability, and adaptability

Relevant key elements

- Natural ventilation
- Solar orientation
- WSUD

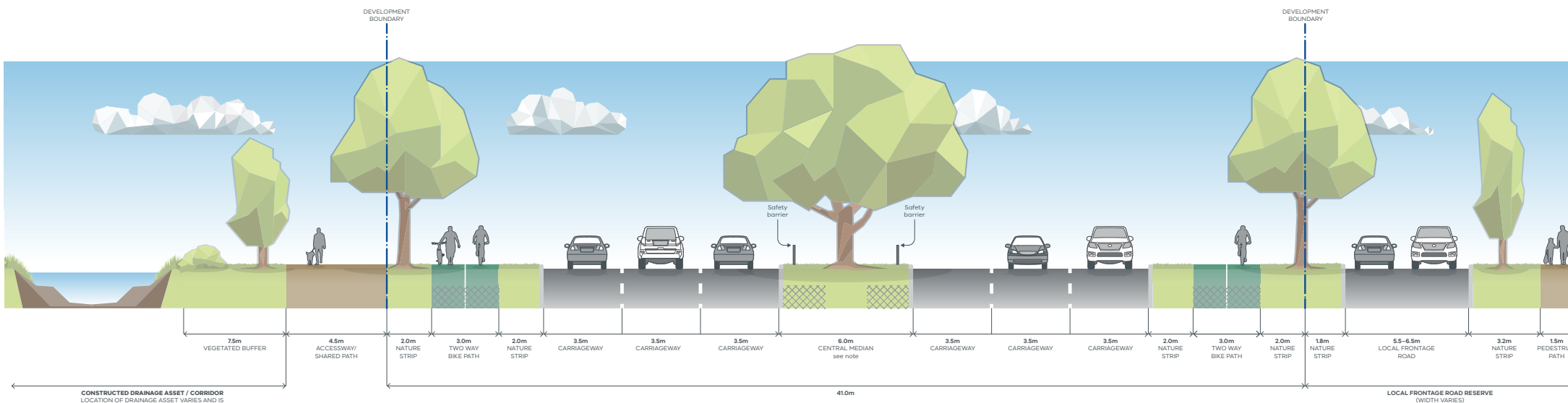
Performance criteria

- The centre must promote the localisation of services which will contribute to a reduction of travel distance to access local services and less dependence on the car
- The centre must include planning and building design which can be adapted to accommodate a variety of uses over time
- WSUD must be integrated to promote the IWM initiatives of the PSP.

Example benchmark outcome

- The selection of retail on the periphery of the centre considers day and evening trading to reduce travel and promote localised services
- Reduced on road car parking promotes the use of alternative transport modes
- Energy efficient design and construction methods for all buildings is investigated
- Water sensitive urban design principles such as integrated stormwater retention and reuse is implemented (e.g. landscape irrigation)
- Options for shade and shelter through a combination of landscape and built form treatments are planned for
- Buildings are naturally ventilated to reduce the reliance on plant equipment for heating and cooling
- Passive solar orientation in the configuration and distribution of built form and public spaces is used to ensure solar access
- Solar energy is capitalised for water and space heating, electricity generation and internal and external lighting
- Floor to ceiling heights allow for adaptive use with a minimum of 3.6m on ground floor
- Public spaces are oriented to capture north sun and protect from prevailing winds and weather.

Primary Arterial 6 Lane – 41m – Officer South Road with waterway interface



NOTES:

- Includes typical commercial / industrial interface both sides.
- Minimum street tree mature height 15 metres.
- Kerbs for arterial carriageways are to be SM2 Semi-Mountable Kerb, and local frontage roads are to be B2 Barrier Kerb.
- See VicRoads *Tree Planting Policy*. Large trees within the road reserve to be protected by safety barriers where required.
- Off-road two-way cycle path shall change to a shared path when traversing under Princes Freeway to allow pedestrian connectivity to the north.
- Provides typical utility services and includes optic fibre conduit and ICP infrastructure for smart city applications.
- 5.5-6.5m wide local frontage road, typical for two-way traffic.
- The carriageway width of local frontage road (e.g. local service road or loop road) shall be appropriate to land use interface, traffic flow and car parking demand, to the satisfaction of the responsible authority.

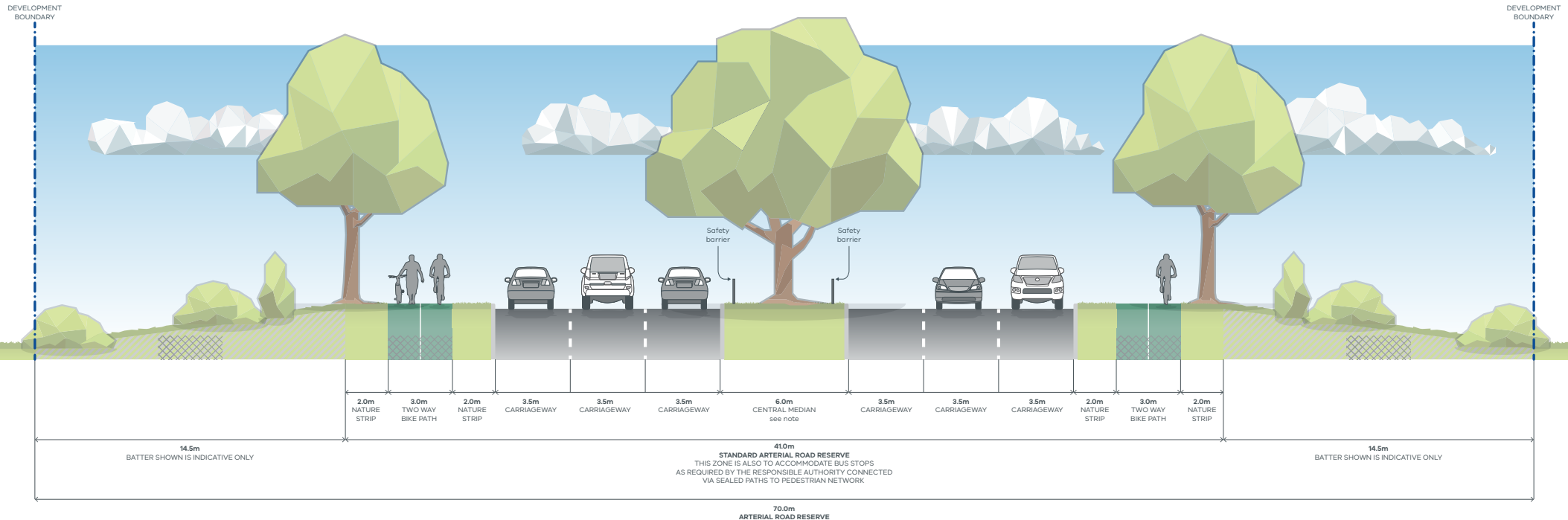


Potential locations for trunk services such as the 900mm diameter sewer main and 750mm diameter recycled water transfer main are indicated with a hatch pattern in the cross sections.

These locations are indicative only and are subject to finalisation post-PSP with the relevant stakeholders. Final location, depth and method of placement shall be to the satisfaction of the relevant authority.

Within tree protection zones, placement of any services must ensure there is minimal to no impact to trees or tree root zones to the satisfaction of the responsible authority.

Primary Arterial 6 Lane – 70m – Thompsons Road



NOTES:

- Includes typical commercial/industrial interface both sides. Local frontage road reserves may provide a service road that connects back to the arterial road in accordance with Department of Transport requirements and standards.
- Minimum street tree mature height 15 metres.
- Kerbs for arterial carriageways are to be SM2 Semi-Mountable Kerb, and local frontage roads are to be B2 Barrier Kerb.
- See VicRoads *Tree Planting Policy*. Large trees within the road reserve to be protected by safety barriers where required.
- Final road reserve width may be reduced and is to be confirmed post-consultation.
- Provide typical utility services and include optic fibre conduit and ICP infrastructure for smart city applications.
- Two-way cycle paths on either side will initially be a shared path, to be converted to a dedicated cycle path once the footpath is provided along the entire length of the adjoining development front.

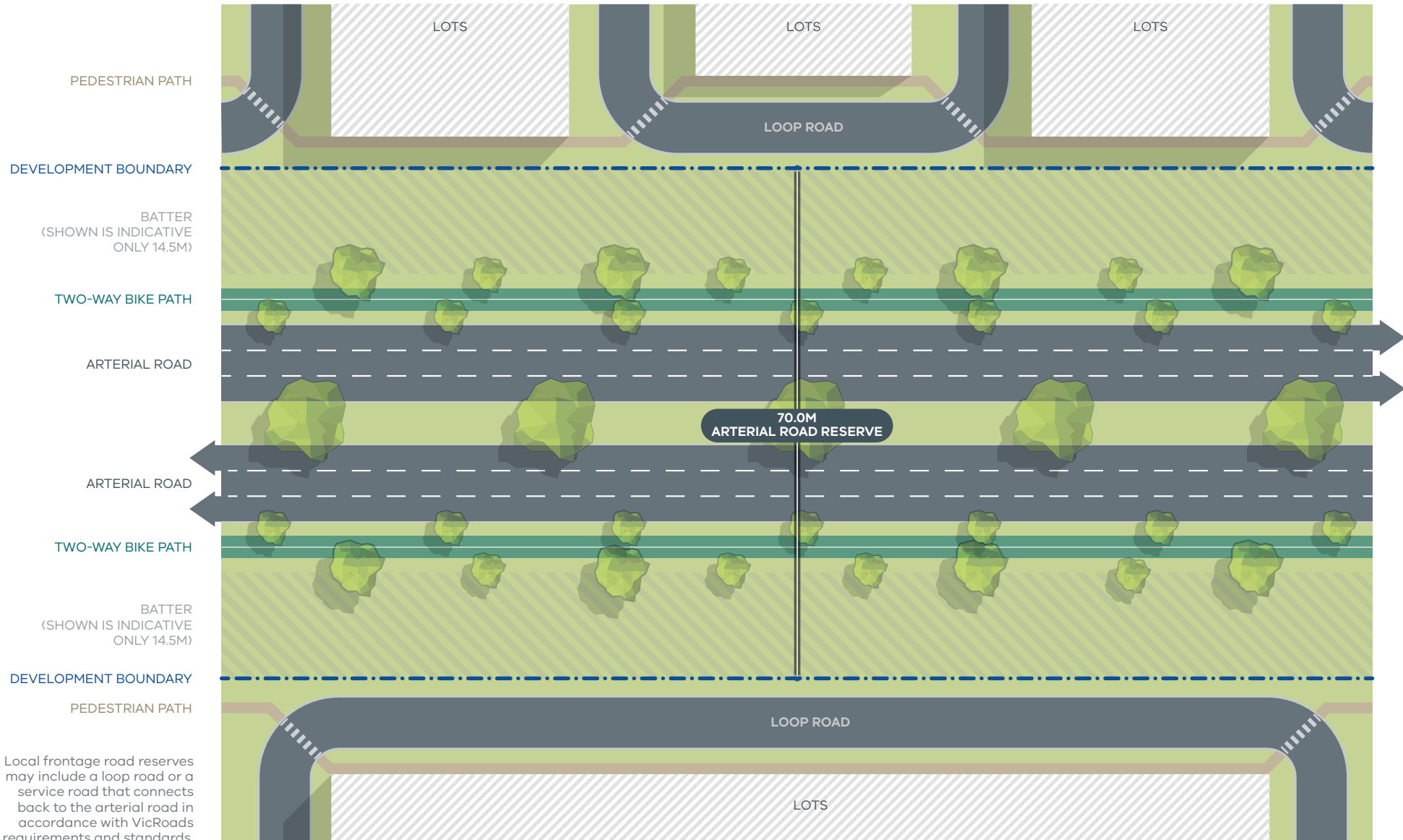


Potential locations for trunk services such as the 900mm diameter sewer main and 750mm diameter recycled water transfer main are indicated with a hatch pattern in the cross sections.

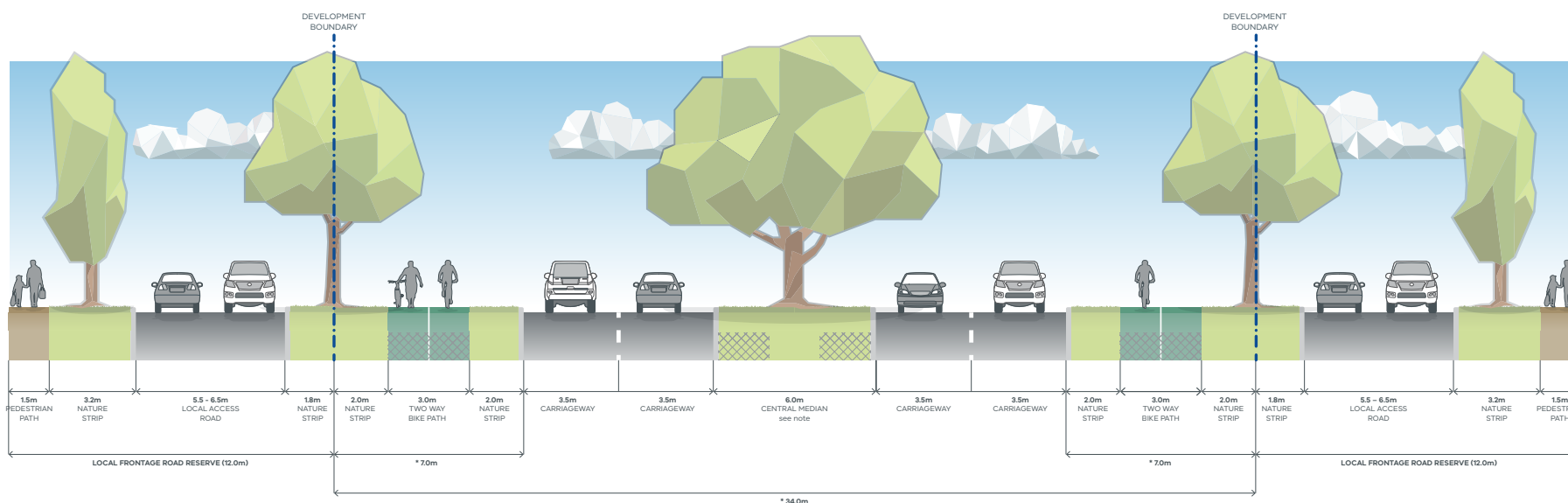
These locations are indicative only and are subject to finalisation post-PSP with the relevant stakeholders. Final location, depth and method of placement shall be to the satisfaction of the relevant authority.

Within tree protection zones, placement of any services must ensure there is minimal to no impact to trees or tree root zones to the satisfaction of the responsible authority.

Primary Arterial 6 Lane – 70m – Thompsons Road – Plan view



Secondary Arterial 4 Lane – 34m – Lecky Road



NOTES:

- Includes typical commercial/industrial interface both sides.
- Minimum street tree mature height 15 metres.
- Kerbs for arterial carriageways are to be SM2 Semi-Mountable Kerb, and local frontage roads are to be B2 Barrier Kerb.
- See VicRoads *Tree Planting Policy*. Large trees within the road reserve to be protected by safety barriers where required.
- * Indicates that road reserve width is to increase where required to include the retention of very high and high value trees identified on the Precinct Features Plan where possible in accordance with Australian Standard AS 4970 2009.
- Two-way cycle paths on either side will initially be a shared path, to be converted to a dedicated cycle path once the footpath is provided along the entire length of the adjoining development front.
- Provides for provision of typical utility services and includes optic fibre conduit and ICP infrastructure for smart city applications.

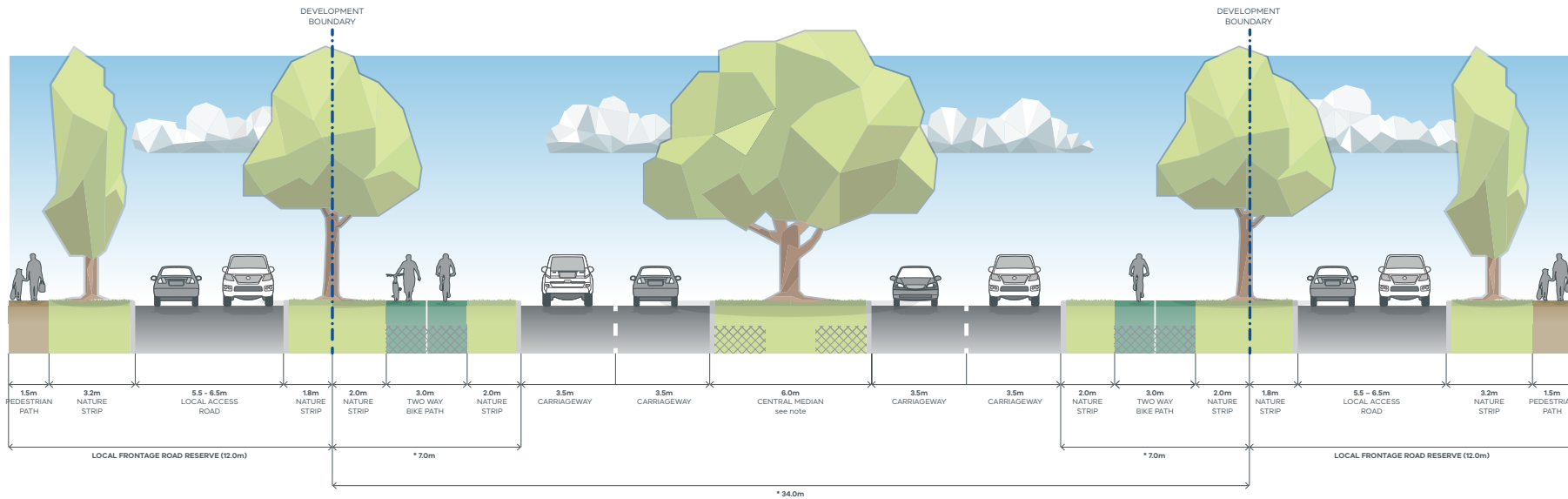


Potential locations for trunk services such as the 900mm diameter sewer main and 750mm diameter recycled water transfer main are indicated with a hatch pattern in the cross sections.

These locations are indicative only and are subject to finalisation post PSP with the relevant stakeholders. Final location, depth and method of placement shall be to the satisfaction of the relevant authority.

Within tree protection zones, placement of any services must ensure there is minimal to no impact to trees or tree root zones to the satisfaction of the responsible authority.

Secondary Arterial 4 Lane – 34m



NOTES:

- Includes typical commercial/industrial interface both sides.
- Minimum street tree mature height 15 metres.
- Kerbs for arterial carriageways are to be SM2 Semi-Mountable Kerb, and local frontage roads are to be B2 Barrier Kerb.
- See VicRoads *Tree Planting Policy*. Large trees within the road reserve to be protected by safety barriers where required.
- * Indicates that road reserve width is to increase where required to include the retention of very high and high value trees identified on the Precinct Features Plan where possible in accordance with Australian Standard AS 4970 2009.
- Two-way cycle paths on either side will initially be a shared path, to be converted to a dedicated cycle path once the footpath is provided along the entire length of the adjoining development front.
- Provides for provision of typical utility services and includes optic fibre conduit and ICP infrastructure for smart city applications.

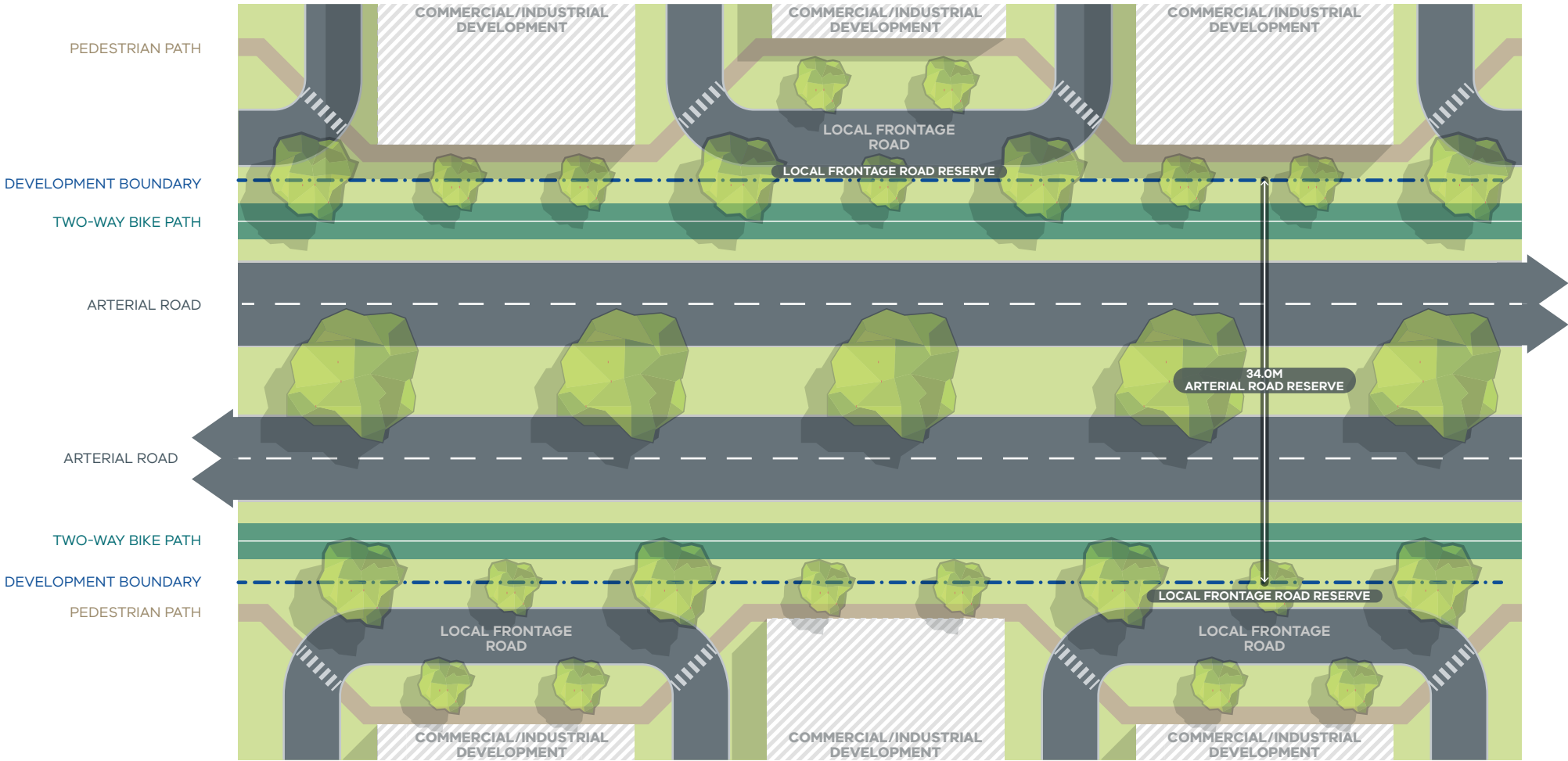


Potential locations for trunk services such as the 900mm diameter sewer main and 750mm diameter recycled water transfer main are indicated with a hatch pattern in the cross sections.

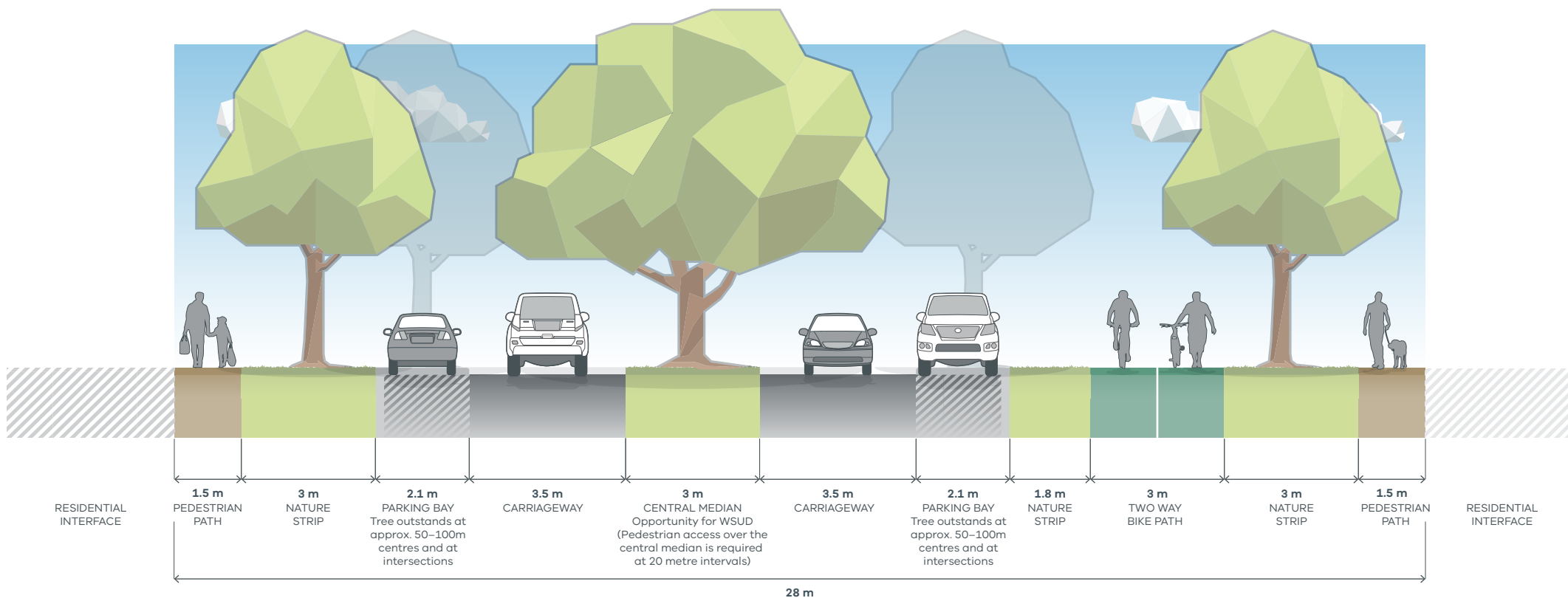
These locations are indicative only and are subject to finalisation post PSP with the relevant stakeholders. Final location, depth and method of placement shall be to the satisfaction of the relevant authority.

Within tree protection zones, placement of any services must ensure there is no impact to trees or tree root zones.

Secondary Arterial 4 Lane – 34m – Plan view



Connector Street Boulevard – 28m – Residential



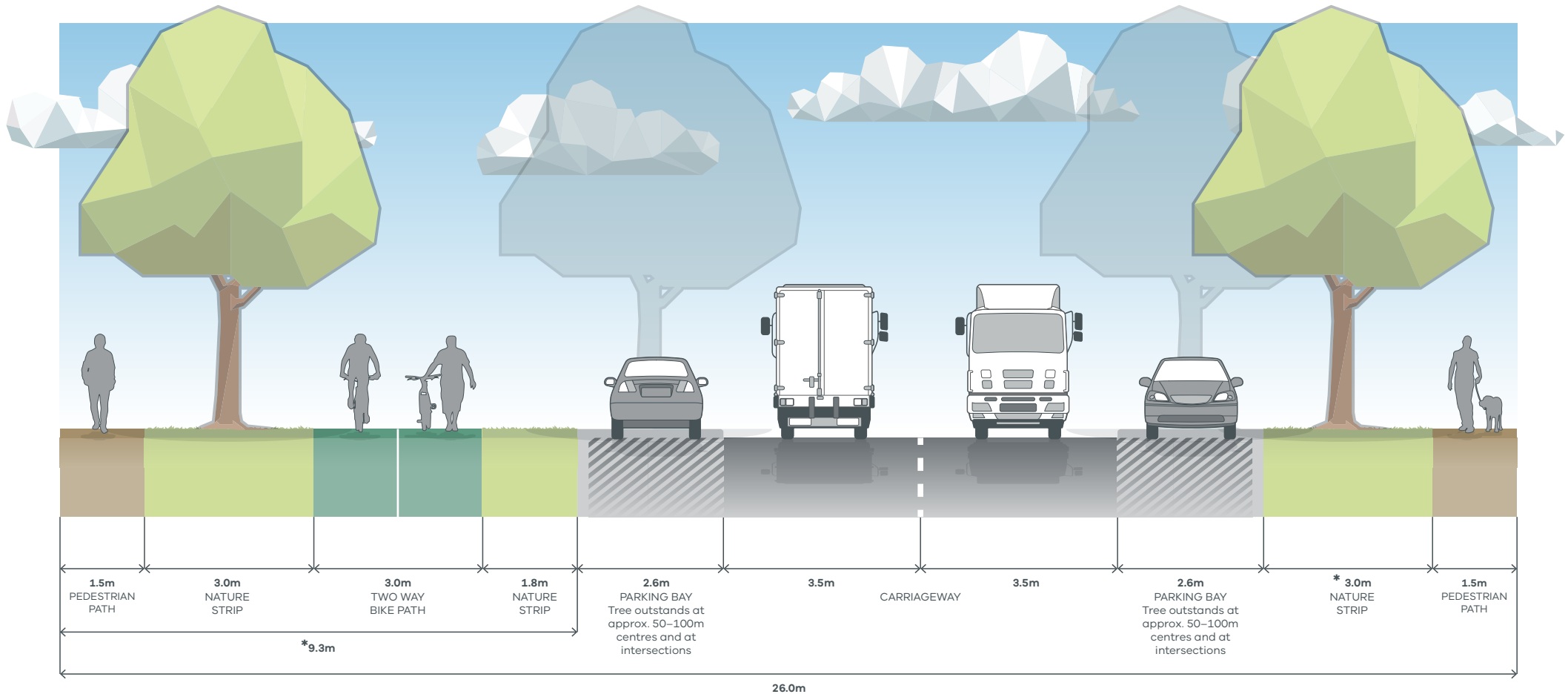
NOTES:

- Include a central median with large canopy trees to create a boulevard effect. Trees are to be centrally planted in median.
- Topsoil used in central medians is to be sandy loam, with a minimum depth of 200mm. The surface of medians is to be free-draining with a minimum cross fall of 2%, and is to be planted with warm season grasses.
- In areas where high pedestrian volumes are expected (e.g. around schools and town centres), central medians should be paved with harder wearing surfaces such as granitic sand or other pavements. Canopy tree planting must be incorporated into additional paved area.
- Any garden beds in central medians are to be offset 1.5m from back of kerb.
- Kerb to central median is to be SM2 semi-mountable kerb.
- Shared paths must appropriately transition to bike paths.

- Depending on the location of breaks in the median, provide intermittent pedestrian crossing points to accommodate mid-block crossings.
- An alternative boulevard treatment can be achieved through a wider verge on one side capable of accommodating a double row of canopy trees.
- Variations to indicative cross-section may include water sensitive urban design (WSUD) outcomes. These could include but are not limited to bioretention tree planter systems and/or median bioretention swales. Such variations must be to the satisfaction of the responsible authority.
- Provides typical utility services and includes optic fibre conduit and ICP infrastructure for smart city applications.
- Accommodate bus stops zone as required by responsible authority connected via sealed paths to pedestrian network.

Connector Street – 26m – Industrial

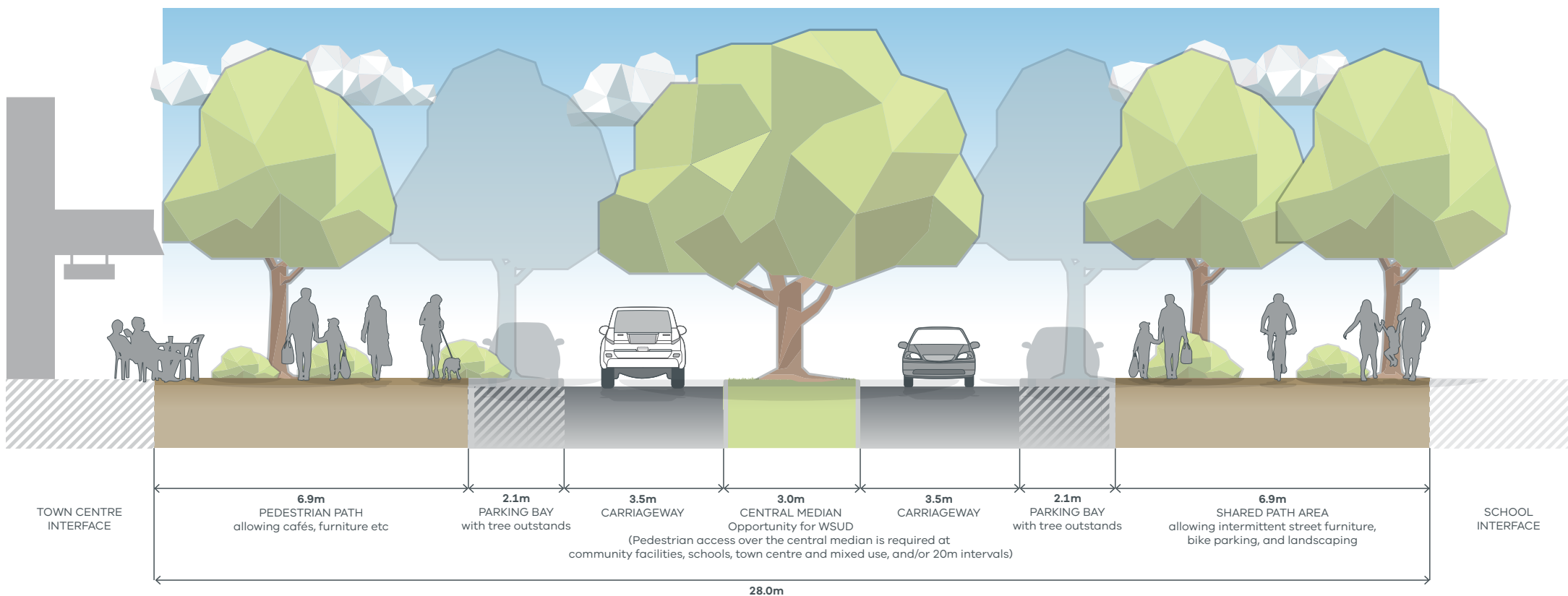
VPA Standard Cross Section



NOTES:

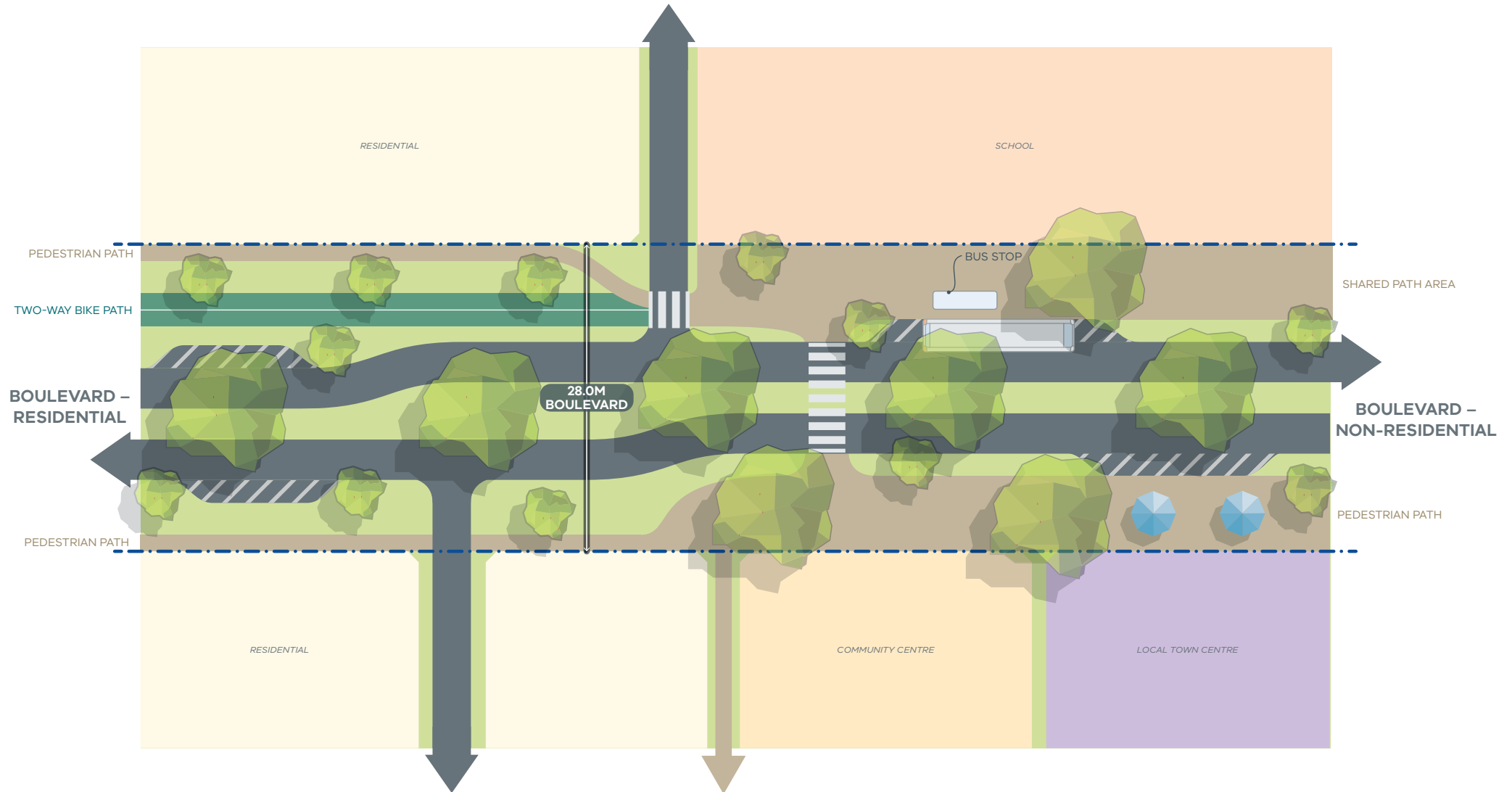
- Minimum street tree mature height 15 metres.
- All kerbs are to be B2 Barrier Kerb.
- Where roads abut thoroughfares, grassed nature strip should be replaced with pavement. Canopy tree planting must be incorporated into any additional pavement.
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority.
- Where a road abuts a local park the pedestrian or shared paths may be located within either the local park or the road reserve.
- Variation to indicative cross section may include water sensitive urban design (WSUD) outcome. These could include but are not limited to bioretention tree planter systems and/or median bioretention swales. Such variations must be to the satisfaction of the responsible authority.
- * Varies indicates that road reserve width is to increase where required to included the retention of very high and high value trees identified on the Precinct Features Plan where possible *in accordance with Australian Standard AS 4970 2009*.
- Provides typical utility services and includes optic fibre conduit and ICP infrastructure for smart city applications.
- Where an industrial connector street is shown on the Public Transport and Active Path Networks Plan without an off-road 2-way cycle path, a 3.0m nature strip and 1.5m pedestrian path will be mirrored on either side of the road carriageways to provide a minimum 21.2m cross section.

Connector Street Boulevard – 28m – Schools & non-residential areas

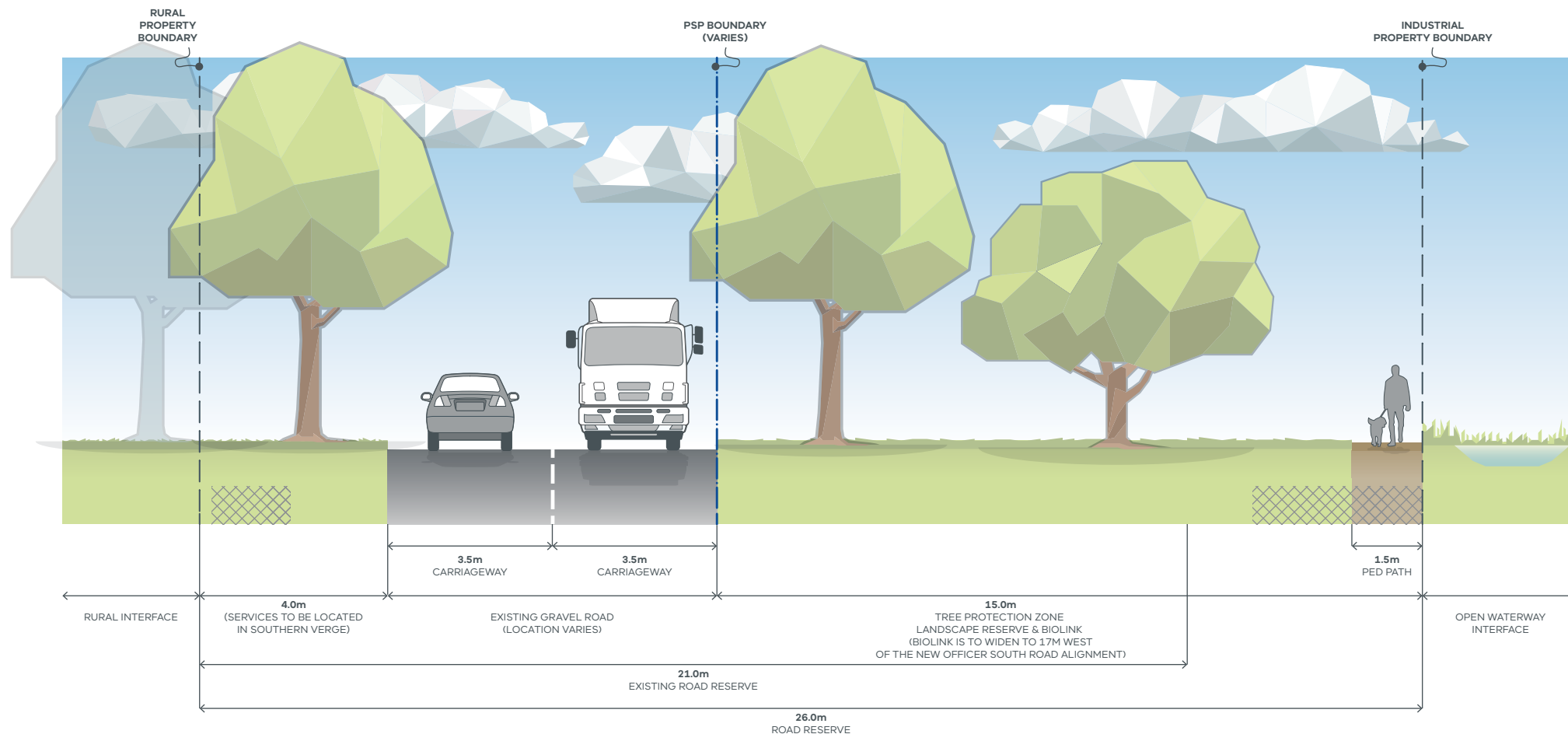


NOTES:

- Include a central median with large canopy trees to create a boulevard effect. Trees are to be centrally planted in median.
- Topsoil used in central medians is to be sandy loam, with a minimum depth of 200mm. The surface of medians is to be free-draining with a minimum cross fall of 2%, and is to be planted with warm season grasses.
- Where pedestrian access is required, the central median should be paved with harder wearing surfaces such as granitic sand or other pavements. Canopy tree planting must be incorporated into additional paved area.
- Kerb to central median is to be SM2 semi-mountable kerb.
- Depending on the location of breaks in the median, provide intermediate pedestrian crossing points to accommodate mid-block crossings.
- Footpaths in the town centre are to be 1.8m minimum width.
- An alternative boulevard treatment can be achieved through a wider verge on one side capable of accommodating a double row of canopy trees.
- Variations to indicative cross-section may include water sensitive urban design (WSUD) outcomes. These could include but are not limited to bioretention tree planter systems and/or median bioretention swales. Such variations must be to the satisfaction of the responsible authority.
- Provide typical utility services and includes optic fibre conduit and ICP infrastructure for smart city applications.
- Accommodate bus stop zones as required by responsible authority connected via sealed paths to pedestrian network.
- The shared path area shall transition to a 1.5m pedestrian path at the end of the school to avoid cycle activity directly at residential frontages. Refer to the boulevard connector transition plan view. A 3m wide shared path is to be provided at other school boundary local access road interfaces.

Boulevard–Connector transition – 28m – Plan view

Patterson Road – 26m – rural interface road adjacent open waterway



NOTES:

- The PSP boundary location varies within the road reserve and in relation to the existing gravel road. The final road pavement and road reserve may vary from this cross section, protecting the biolink as the main focus of the road reserve design.
- To provide suitable access, the existing gravel road located both internal and external to the precinct boundary, is to be sealed to the satisfaction of the relevant authority.
- Existing trees within the biolink and very high to high value trees identified on the Precinct Features Plan should be retained where practical and to the satisfaction of the responsible authority.
- Where roads abut thoroughfares, grassed nature strip should be replaced with pavement. Canopy tree planting must be incorporated into any additional pavement.
- Provide typical utility services and include optic fibre conduit and ICP infrastructure for smart city applications.

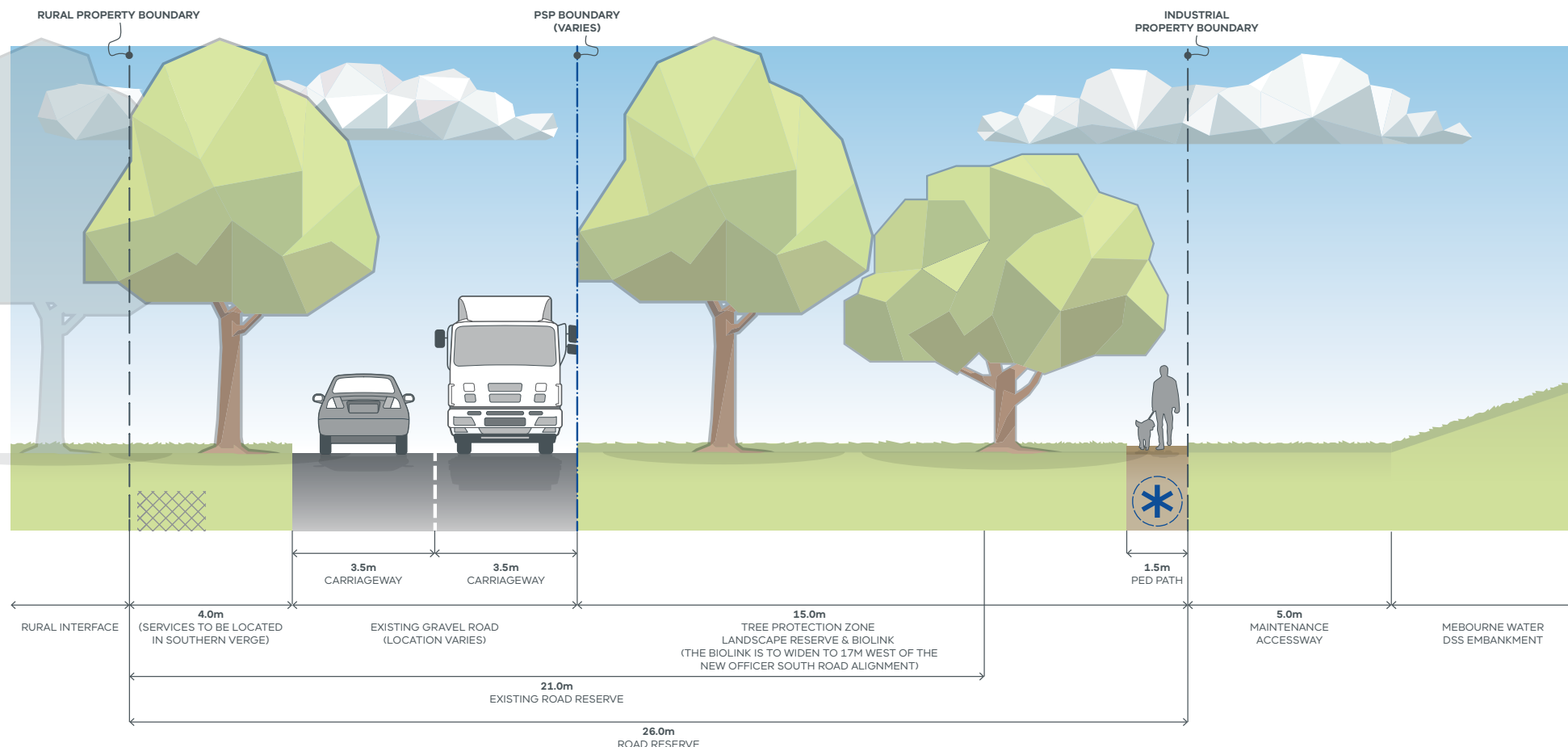
- An access way for maintenance purposes is required within the waterway reserve interfacing with the biolink. Melbourne Water in consultation with council will undertake a functional design of this constructed waterway to ensure waterway functions, maintenance, biolink requirements and sodic soil risks are applied. In relation to this waterway and the subdivision interface, refer to G37. For all the other PSP constructed waterways, refer to *Typical open waterway interface* cross section in Appendix 7.



Potential locations for trunk services such as the 900mm diameter sewer main and 750mm diameter recycled water transfer main are indicated with a hatch pattern in the cross sections. These locations are indicative only and are subject to finalisation post-PSP with the relevant stakeholders. Final location, depth and method of placement shall be to the satisfaction of the relevant authority.

Within tree protection zones, placement of any services must ensure there is minimal to no impact to trees or tree root zones to the satisfaction of the responsible authority.

Patterson Road – 26m – rural interface road adjacent retarding basin



NOTES:

- The PSP boundary location varies within the road reserve and in relation to the existing gravel road. The final road pavement and road reserve may vary from this cross section, protecting the biolink as the main focus of the road reserve design.
- To provide suitable access, the existing gravel road located both internal and external to the precinct boundary, is to be sealed to the satisfaction of the relevant authority.
- Existing trees within the biolink and very high to high value trees identified on the Precinct Features Plan should be retained where practical and to the satisfaction of the responsible authority. No works are to occur within the Tree Protection Zone as per the Australian standards for tree protection unless to the satisfaction of the responsible authority.
- Provide typical utility services and include optic fibre conduit and ICP infrastructure for smart city applications.



1500mm diameter Melbourne Water overflow pipe to be located under the pedestrian path for the last 100m of the road reserve, subject to further functional and detailed design to the satisfaction of Melbourne Water and responsible authorities. The biolink trees must be protected to the satisfaction of Cardinia Council.

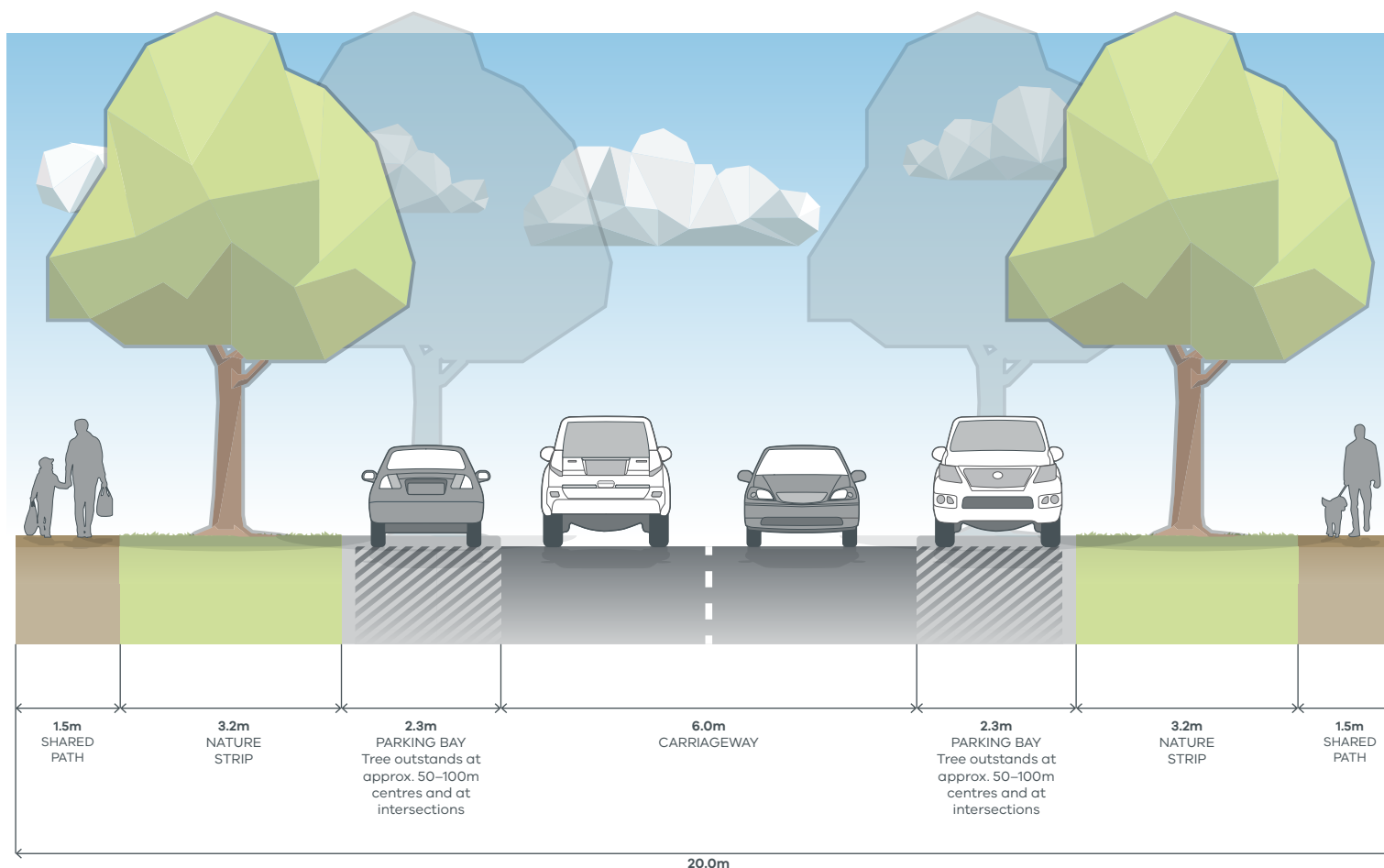


Potential locations for trunk services such as the 900mm diameter sewer main and 750mm diameter recycled water transfer main are indicated with a hatch pattern in the cross sections. These locations are indicative only and are subject to finalisation post-PSP with the relevant stakeholders. Final location, depth and method of placement shall be to the satisfaction of the relevant authority.

Within tree protection zones, placement of any services must ensure there is minimal to no impact to trees or tree root zones to the satisfaction of the responsible authority.

Local Access Street Level 2 – 20m – Residential

VPA Standard Cross Section

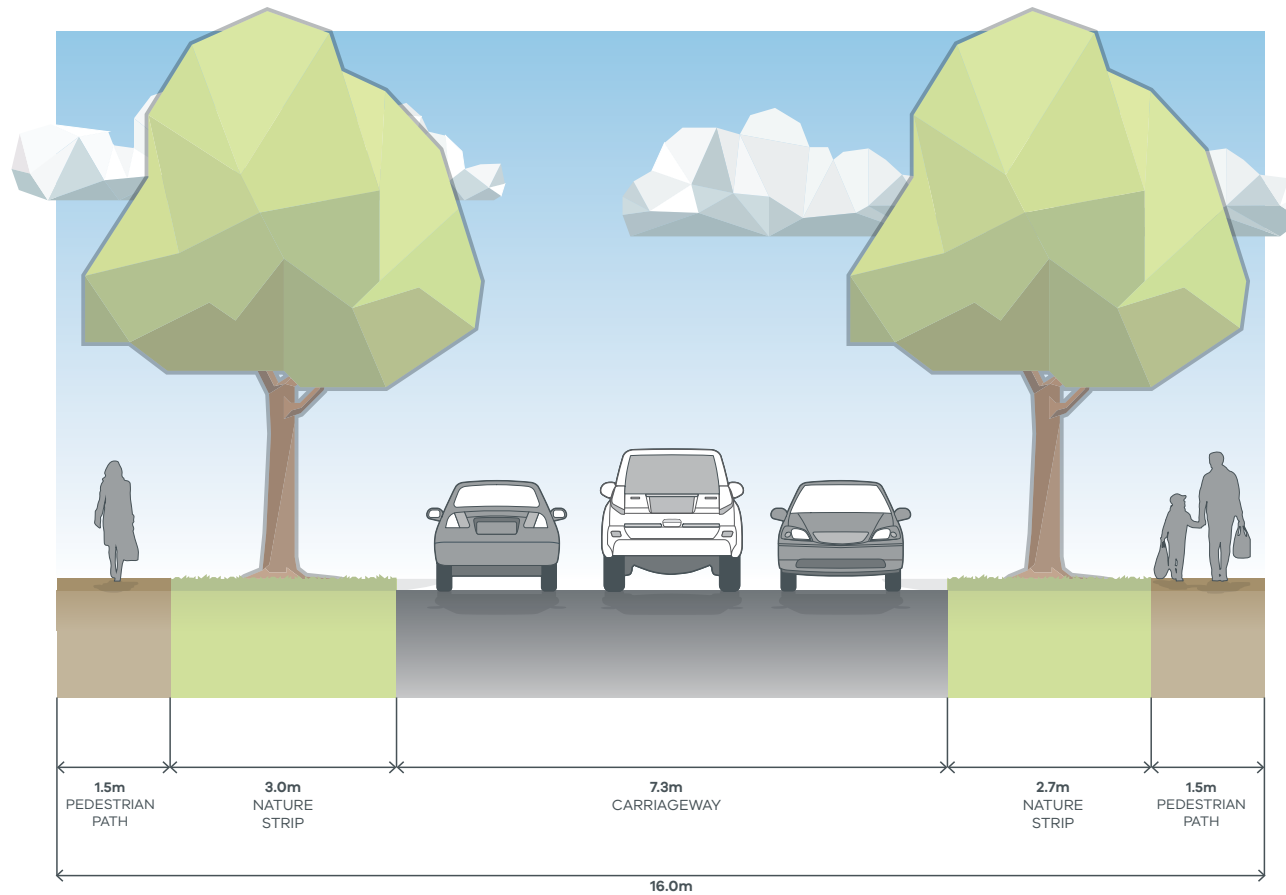


NOTES:

- Minimum street tree mature height 12 metres
- All kerbs are to be B2 Barrier Kerb.
- Verge widths may be reduced where roads abut open space with consent of the responsible authority.
- Where a road abuts a local park the pedestrian or shared paths may be located within either the local park or the road reserve.

Local access street Level 1 (16m) – Residential

VPA Standard Cross Section

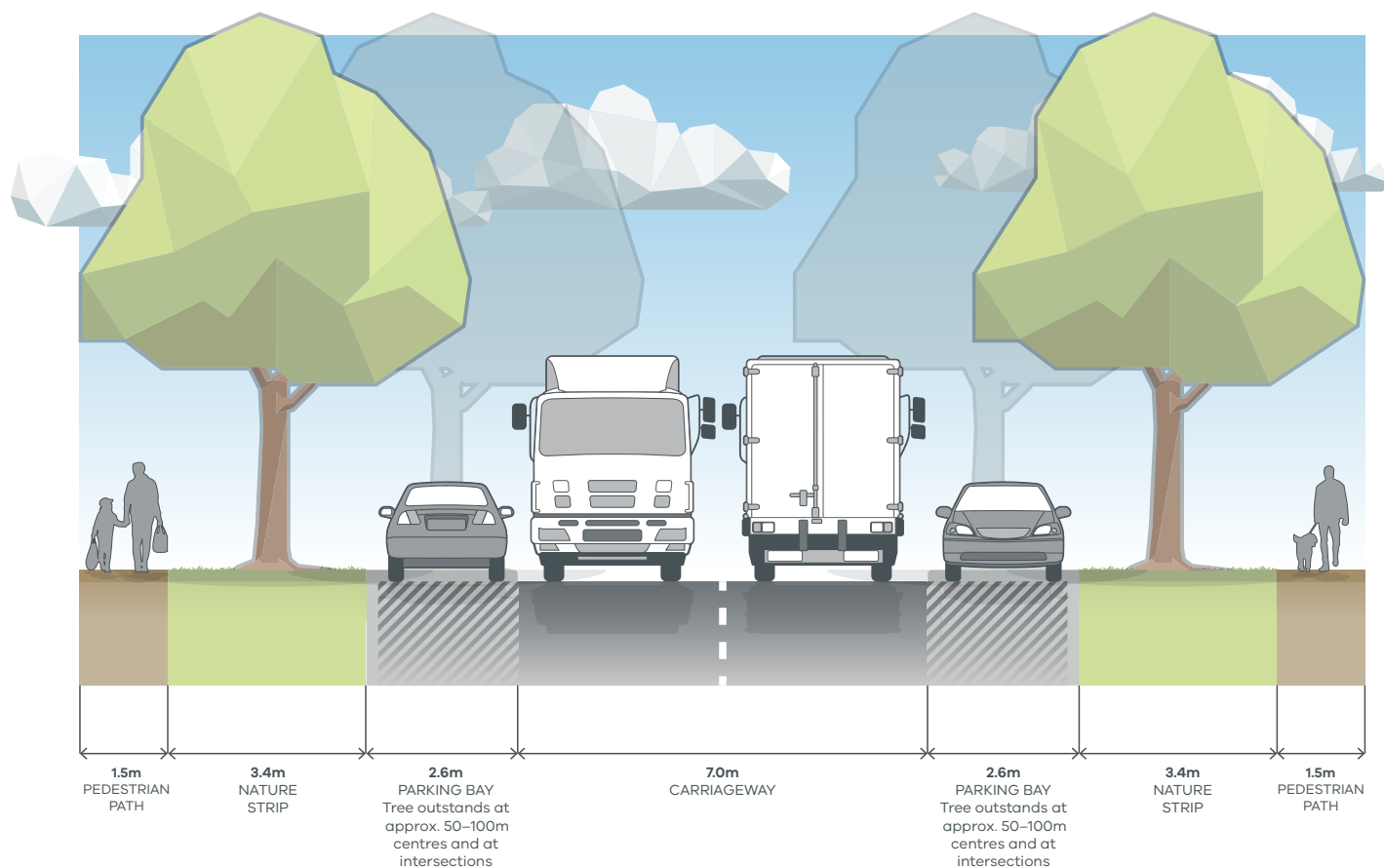


NOTES:

- Minimum street tree mature height 15 metres.
- All kerbs are to be B2 Barrier Kerb..
- Verge widths may be reduced where roads abut open space with consent of the responsible authority.

Local Access Street Level 2 – Industrial (22m)

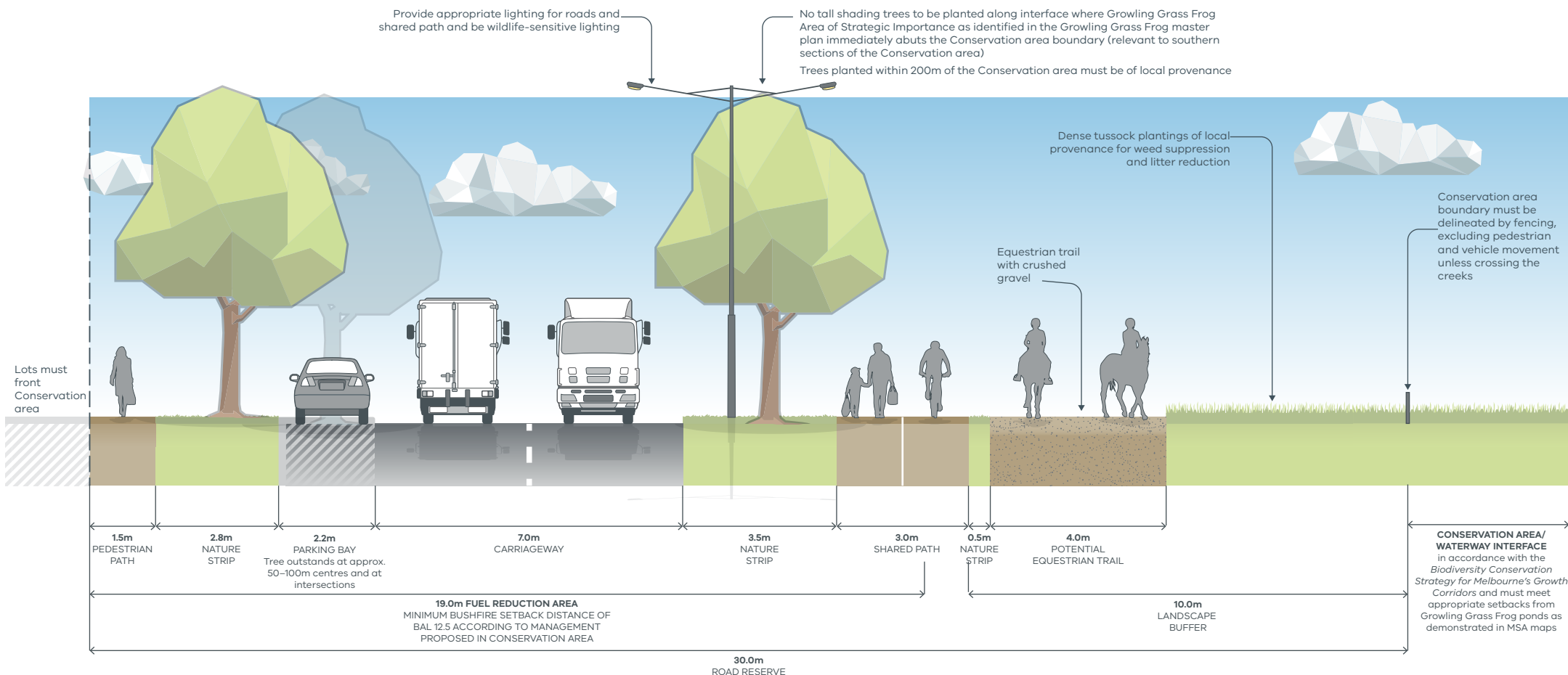
VPA Standard Cross Section



NOTES:

- All kerbs are to be B2 Barrier Kerb.
- Verge widths may be reduced where roads abut open space with consent of the responsible authority.

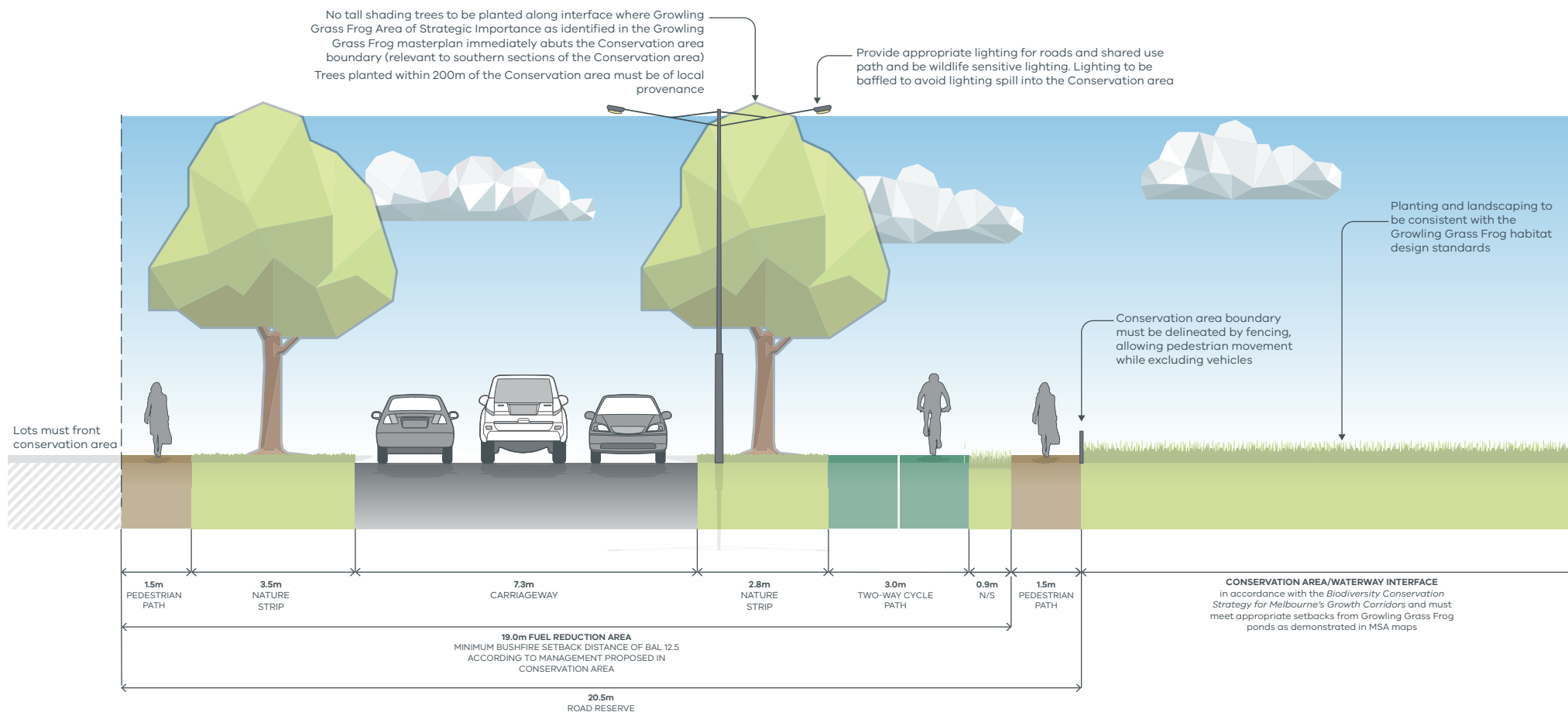
Local Access Street industrial – CVIA and Conservation area



NOTES:

- Street tree planting of a medium to large size appropriate for the widths and function of the street.
- All kerbs are to be B2 Barrier Kerb.
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority.
- All necessary fire breaks must be outside of the Conservation area.
- Where a road abuts a local park the pedestrian or shared paths may be located within either the local park or the road reserve.
- Fencing must be constructed in accordance with the recommendations in the DEECA requirements for permanent fencing around conservation areas under the Melbourne Strategic Assessment.
- Street lights installed in the nature strip must provide lighting for both the road and shared path and be wildlife sensitive lighting to assist reducing light spill into the Conservation area, in accordance with the MSA Growling Grass Frog Crossing Design Standards.
- Nature strip trees must be indigenous species within 200m of Conservation areas, in accordance with the Cardinia Shire Council Developer Landscape Guidelines.
- Space must be provided for off-road vehicle pull-over bays at maintenance access points (minimum 12m long x 2.1m wide).
- Provides typical utility services and includes optic fibre conduit and ICP infrastructure for smart city applications.

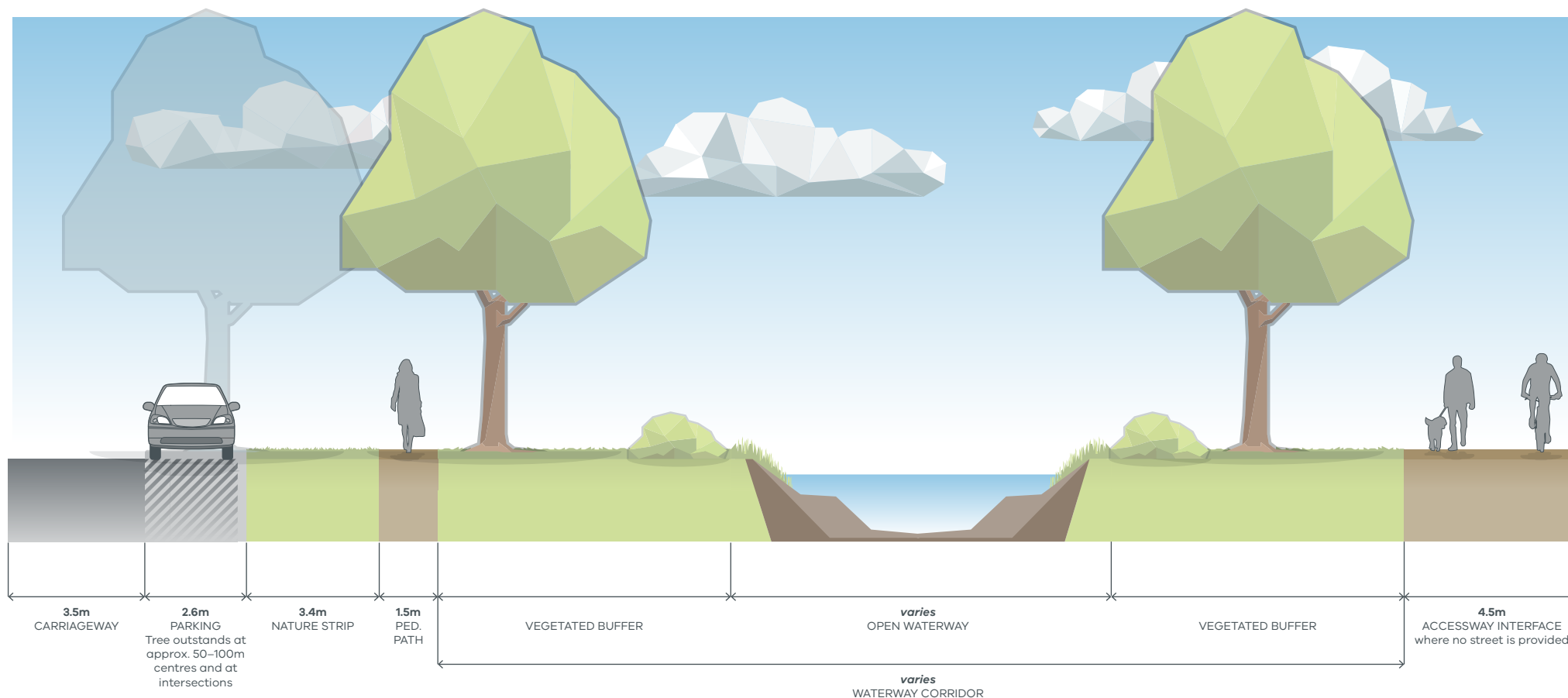
Local Access Street residential – conservation interface



NOTES:

- Street tree planting of a medium to large size appropriate for the widths and function of the street.
- All kerbs are to be B2 Barrier Kerb.
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority.
- All necessary fire breaks must be outside of the conservation area.
- Where a road abuts a local park the pedestrian or shared paths may be located within either the local park or the road reserve.
- Fencing must be constructed in accordance with the recommendations in the *DEECA requirements for permanent fencing around conservation areas under the Melbourne Strategic Assessment*.
- Street lights installed in the nature strip must provide lighting for both the road and shared path and be wildlife sensitive lighting to assist reducing light spill into the Conservation Area, in accordance with the *MSA Growling Grass Frog Crossing Design Standards*.
- Nature strip trees must be indigenous species within 200m of Conservation areas, in accordance with the *Cardinia Shire Council Developer Landscape Guidelines*.
- Space must be provided for off-road vehicle pull-over bays at maintenance access points (minimum 12m long x 2.1m wide).
- Provides typical utility services and includes optic fibre conduit and ICP infrastructure for smart city applications.

Open waterway interface to utility easements



NOTE:

- Shared pathways (including equestrian trails) and infrastructure is to be located outside the waterway corridor.
- This is a typical cross section for the constructed open waterways interfacing with a utility easement **excluding** Lower Gum Scrub Creek and the Patterson Road biolink waterway which addresses protected tree root zones.
- A 4.5m wide access way is to be included within the active edge zone for use as a maintenance track where no street interface is proposed. This can be co-used as a shared path or equestrian trail.

Appendix 8 Design and location of underground services

The *Engineering Design and Construction Manual for Subdivision in Growth Areas* (December 2019) outlines placement of services for a typical residential street environment. This approach is appropriate for most of the 'standard' road cross sections outlined in [Appendix 7 Standard road cross sections & functional layout plans](#), containing grassed nature strips, footpaths and road pavements.

Non-standard road cross sections

To achieve greater diversity of streetscape outcomes in Melbourne's growth areas, which enhances character and amenity of these new urban areas, non-standard road cross sections are required. Non-standard road cross sections will also be necessary to address local needs, such as fully sealed verges for high pedestrian traffic areas in town centres and opposite schools. This PSP contains suggested non-standard 'variation' road cross sections, however other non-standard outcomes are encouraged.

For non-standard road cross sections where service placement guidance outlined in Figure 003 and 004 in the *Engineering Design and Construction Manual for Subdivision in Growth Areas* (December 2019) is not applicable, the following service placement guidelines will apply, if being installed.

General principles for service placement:

- Place gas and water on one side of road, electricity on the opposite side
- Place water supply on the high side of road
- Place services that need connection to adjacent properties closer to these properties
- Place trunk services further away from adjacent properties
- Place services that relate to the road carriageway (e.g. drainage, street light electricity supply) closer to the road carriageway
- Maintain appropriate services clearances and overlap these clearances wherever possible.

Table 9 Design and location of underground services

	UNDER PEDESTRIAN PAVEMENT	UNDER NATURE STRIP	DIRECTLY UNDER TREES	UNDER KERB	UNDER ROAD PAVEMENT	WITHIN ALLOTMENT	NOTES
SEWER	Preferred	Possible	Possible	No	Possible	Possible	
POTABLE WATER	Possible	Preferred	Preferred	No	No	No	Can be placed in combined trench with gas
RECYCLED WATER	Possible	Preferred	Preferred	No	No	No	
GAS	Possible	Preferred	Preferred	No	No	No	Can be placed in combined trench with potable water
ELECTRICITY	Preferred	Possible	Possible	No	No	No	Pits to be placed either fully in footpath or nature strip
FTTH/TELCO	Preferred	Possible	Possible	No	No	No	Pits to be placed either fully in footpath or nature strip
DRAINAGE	Possible	Possible	Possible	Possible	Possible	Possible	
TRUNK SERVICES	Possible	Possible	Possible	Possible	Possible	No	

Notes:

- Placement of services under road pavement is to be considered when service cannot be accommodated elsewhere in road reserve.
- Placement of services beneath edge of road pavement/parking bays is preferable to within traffic lanes.
- Services underneath pedestrian pavements may be considered where allotment size/frontage width allows adequate room to access and work on a pipe where connections to properties are within a pit.

Appendix 9 Glossary of terms

TERM	DEFINITION
activity centre	Provide the focus for services, employment and social interaction. They are where people shop, work, meet, relax and live. Usually well-served by public transport, they range in size and intensity of use.
affordable housing	Has the same meaning as Section 3AA of the <i>Planning and Environment Act 1987</i> .
Biodiversity Conservation Strategy (BCS)	<i>The Biodiversity Conservation Strategy for Melbourne Growth Corridors</i> (State Government of Victoria, 2013).
biodiversity sensitive urban design	A protocol with the aim of transforming urban areas to have increased habitat and food resources for native species and ecological communities.
canopy cover	The total area that all canopy tree foliage covers within the public realm.
canopy tree	A tree which has a potential canopy of foliage larger than 6m in diameter at maturity in the summer months.
CHMP	Cultural Heritage Management Plan. A written report prepared by a Heritage Advisor. It includes an assessment of the potential impact of a proposed activity on Aboriginal cultural heritage. It outlines measures to be taken before, during and after an activity in order to manage and protect Aboriginal cultural heritage in the activity area.
co-location	Adjoining land uses to enable complementary programs, activities, and services as well as shared use of resources and facilities, for example, the co-location of schools and sporting fields.
community facilities	<p>Infrastructure provided by government or non-government organisations for accommodating a range of community support services, programs, and activities. This includes:</p> <ul style="list-style-type: none"> • Facilities for education and learning (e.g. government and non-government schools, universities, adult learning centres) • Early years (e.g. preschool, maternal and child health, childcare) • Health and community services (e.g. hospitals, aged care, doctors, dentists, family and youth services, specialist health services) • Community (e.g. civic centres, libraries, neighbourhood houses) • Arts and culture (e.g. galleries, museums, performance space) • Sport, recreation and leisure (e.g. swimming pools) • Justice (e.g. law courts) • Voluntary and faith (e.g. places of worship) and • Emergency services (e.g. police, fire and ambulance stations).
encumbered land	<p>Land that is constrained for development purposes, including:</p> <ul style="list-style-type: none"> • Easements for power/transmission lines, sewer, gas, waterways/drainage • Retarding basins/ wetlands • Landfill • Conservation, protected vegetation, and • Heritage areas. <p>This land may be used for a range of activities (e.g. walking trails, sports fields) and is not credited. However, regard is taken to the availability of encumbered land when determining the open space requirement.</p>
fire threat edge	The interface between urban development and an area which presents a permanent potential for fire to impact on a community.
frontage	The road alignment at the front of a lot. If a lot abuts two or more roads, the one to which the building, or proposed building faces.
gross developable area	Total precinct area excluding encumbered land, arterial roads and other roads with four or more lanes.
high density housing	Housing with an average density of 40 dwellings or more per net developable hectare.

TERM	DEFINITION
housing density (gross)	The number of houses divided by gross developable area.
housing density (net)	The number of houses divided by net developable area.
key worker housing	Affordable rental housing that is appropriate for people who work within Cardinia Shire Council, who require a physical presence to perform their work, and whose household earns very low, low or moderate incomes. The housing must be allocated and monitored by a Registered Housing Agency.
labour hire accommodation	Any house, building, structure or site (including any shared facilities on a site), whether temporary or permanent, used to provide accommodation for a worker under, or in connection with, a labour hire arrangement with that worker. It applies to four or more workers.
land use budget table	A table setting out the total precinct area, gross developable area, net developable area and constituent land uses proposed within the precinct.
linear open space network	Corridors of open space, mainly along waterways that link together forming a network.
local town centre	Activity centres that are an important community focal point and have a mix of uses to meet local needs. Accessible to a viable user population by walking, cycling and by local bus services and public transport links to one or more principal or major activity centres. This should be sufficient size to accommodate a supermarket.
main street	A function of an activity centre, where vitality and activity are created by orienting uses towards the street and ensuring that the primary address of all retail stores is the street. This would normally be a connector street rather than an arterial road.
medium density housing	Housing with an average density of 35–39 dwellings per net developable hectare.
net developable area	Land within a precinct available for development. This excludes encumbered land, arterial roads, railway corridors, government schools and community facilities and public open space. It includes lots, local streets, and connector streets. Net Developable Area may be expressed in terms of hectare units (i.e. NDHa).
passive open space	Open space that is set aside for parks, gardens, linear corridors, conservation bushlands, nature reserves, public squares and community gardens that are made available for passive recreation, play and unstructured physical activity including walking, cycling, hiking, revitalisation, contemplation and enjoying nature.
principal freight network	A strategic network of current and recognised future freight places and connecting movement corridors (road and rail) which are nationally, state and/or regionally significant.
principal public transport network	Incorporated document <i>Principal Public Transport Network 2017</i> (Victorian Government, 2017), and <i>Principal Public Transport Network Area Maps</i> (Victorian Government, August 2018).
public open space	Land set aside in the precinct structure plan for public recreation that incorporates active and passive open space.
sensitive response	A design or engineered response that does not significantly contrast with the existing landform.
social housing	A type of rental housing that is provided and/or managed by the government or by a not-for-profit organisation. Social housing is an overarching term that covers both public housing and community housing.
temporary crisis accommodation	Temporary crisis accommodation provided on a non-profit basis by an agency which receives homelessness support funding from the State.
unencumbered	Land that is not constrained by uses required to enable development (including easements for power/transmission lines, sewer, gas, waterways/drainage; retarding basins/wetlands; landfill; conservation protection vegetation and heritage areas).
water sensitive urban design	A sustainable water management approach that aims to provide water-quality, flood management and green landscapes. Key principles include minimising water-resistant areas; recharging natural groundwater aquifers (where appropriate) by increasing the amount of rain absorbed into the ground; encouraging onsite reuse of rain and incorporation of rain gardens encouraging onsite treatment to improve water quality and remove pollution and using temporary rainfall storage (e.g., basins/wetlands) to reduce the load on drains.

Appendix 10 Recommended tree inclusion on Plans 2 and 7

[Plan 2 Precinct Features](#)
[Plan 7 Native Vegetation Retention and Removal](#)

Table 10 345 Officer South Road

HOMEWOOD CONSULTING NUMBER	TREE SHOULD BE INCLUDED ON PLAN 2	REASON	TREE SHOULD BE INCLUDED ON PLAN 7 AS:		TREE SHOULD NOT BE INCLUDED ON PLAN 7 (NOT NATIVE TO VICTORIA)
			"NATIVE TREE TO BE RETAINED"	"NATIVE TREE TO BE RETAINED, IF POSSIBLE, TO THE SATISFACTION OF THE RESPONSIBLE AUTHORITY"	
28–36, 38, 39, 41	Yes	Trees of fair or good health and structure with long ULE	No	Yes (planted)	N/A
18–27, 37	Yes	Trees of fair or good health and structure with long ULE	No	No	Yes
17	No	Not suitable for retention in areas accessed by public	N/A	N/A	N/A
43–47, 50–64	No	Starting to break up	N/A	N/A	N/A
48, 49, 65–69	No	Break up of adjacent trees 50–64 likely to lead to changes in wind dynamics that will impact structural integrity of these trees	N/A	N/A	N/A

Table 11 425 Officer South Road

HOMEWOOD CONSULTING NUMBER	TREE SHOULD BE INCLUDED ON PLAN 2	REASON	TREE SHOULD BE INCLUDED ON PLAN 7 AS:		TREE SHOULD NOT BE INCLUDED ON PLAN 7 (NOT NATIVE TO VICTORIA)
			"NATIVE TREE TO BE RETAINED"	"NATIVE TREE TO BE RETAINED, IF POSSIBLE, TO THE SATISFACTION OF THE RESPONSIBLE AUTHORITY"	
85, 121	Yes	Remnant tree in good condition	Yes		
88, 92, 94–99, 102–113, 128–150, 154–162, 172, 178, 190–199, 202–207	Yes	Trees of fair or good health and structure with long ULE	No	Yes (planted)	N/A
70–72, 86, 87, 89–91, 151–153, 163–171, 174–177, 179–183, 186–189, 208, 210, 211	Yes	Trees of fair or good health and structure with long ULE	No	No	Yes
93, 100, 101, 200, 201	Only if confirmed habitat tree	Habitat value to be confirmed by ecologist	No	Only if confirmed habitat tree	N/A
173, 184, 185	No	All <i>Pinus radiata</i> , a species recognised by Cardinia Shire Council as an environmental weed	N/A	N/A	N/A
209	No	In decline	N/A	N/A	N/A
122, 123	No	Starting to break up	N/A	N/A	N/A

***PSP*2.0**

Officer South (Employment)

BUNURONG COUNTRY

Precinct Structure Plan

NOVEMBER 2024